



EASTLAKE TOWNHOUSES

2236 FAIRVIEW AVE E
2239 MINOR AVE E
2247 MINOR AVE E
67 E LYNN ST

3024310-EG

DESIGN REVIEW RECOMMENDATION MEETING

MEETING DATE:
MARCH 13TH, 2019
8 pm

ADDRESS:
GARFIELD COMMUNITY CENTER
2323 E CHERRY STREET-MULTIPURPOSE ROOM
SEATTLE, WA 98122

HYBRID

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Table of Contents

DESIGN OVERVIEW

- 2 Team / Development Objectives
- 3 Project Site Information

CONTEXT & SITE ANALYSIS

- 4 Site Context
- 5 Context and Urban Analysis
- 6 Zoning and Project Information
- 7-9 Street Montages
- 10 Context Images
- 11 Site Survey

DESIGN EVOLUTION

- 12 EDG Summary
- 13-14 EDG Guidance
- 15-25 Massing and Architectural Concept
- 26-30 Residential Entries
- 31-37 Open Space and Street Engagement
- 38-39 Recycling and Trash Enclosures

CONCEPT DEVELOPMENT

- 40-44 Plans
- 45 Amenity Space
- 46 Landscape Plan
- 47 Lighting Plan
- 48 Wayfinding & Signage
- 49-53 Elevations
- 54 Shadow Analysis
- 55-59 Departures
- 60 Response to Public Comment
- 61 EDG Response Summary
- 62 Previous Relevant Projects

Thank You

Development Objectives

*Provide townhouses with unique and specific to
Create project with strong sense of community.*

This project proposes 27 rowhouse and townhouse units with parking for 27 vehicles located in the Eastlake neighborhood near the shoreline of Lake Union. As this development increases the density of the Eastlake neighborhood, the project aims to respect the existing residential scale through appropriate massing, proportion and materials. Along the perimeter of the three surroundingn streets (Minor Ave E, E Lynn St and Fairview Ave E, the new homes will engage with street facing entries and stoops to activate the pedestrian realm. Internally, the project aimes to create a micro-community between structures, through a woonerf and pocket park. The project will address the existing vehicular and pedestrian traffic patterns of the neighborhood and add a variety of materials and forms to compliment the diverse residential community that defines Eastlake.

Let Us Introduce Ourselves

Rob Humble
Architect

Barrett Eastwood
Architect

Gina Gage
Project Manager

Andres Salazar Del Pozo
Designer

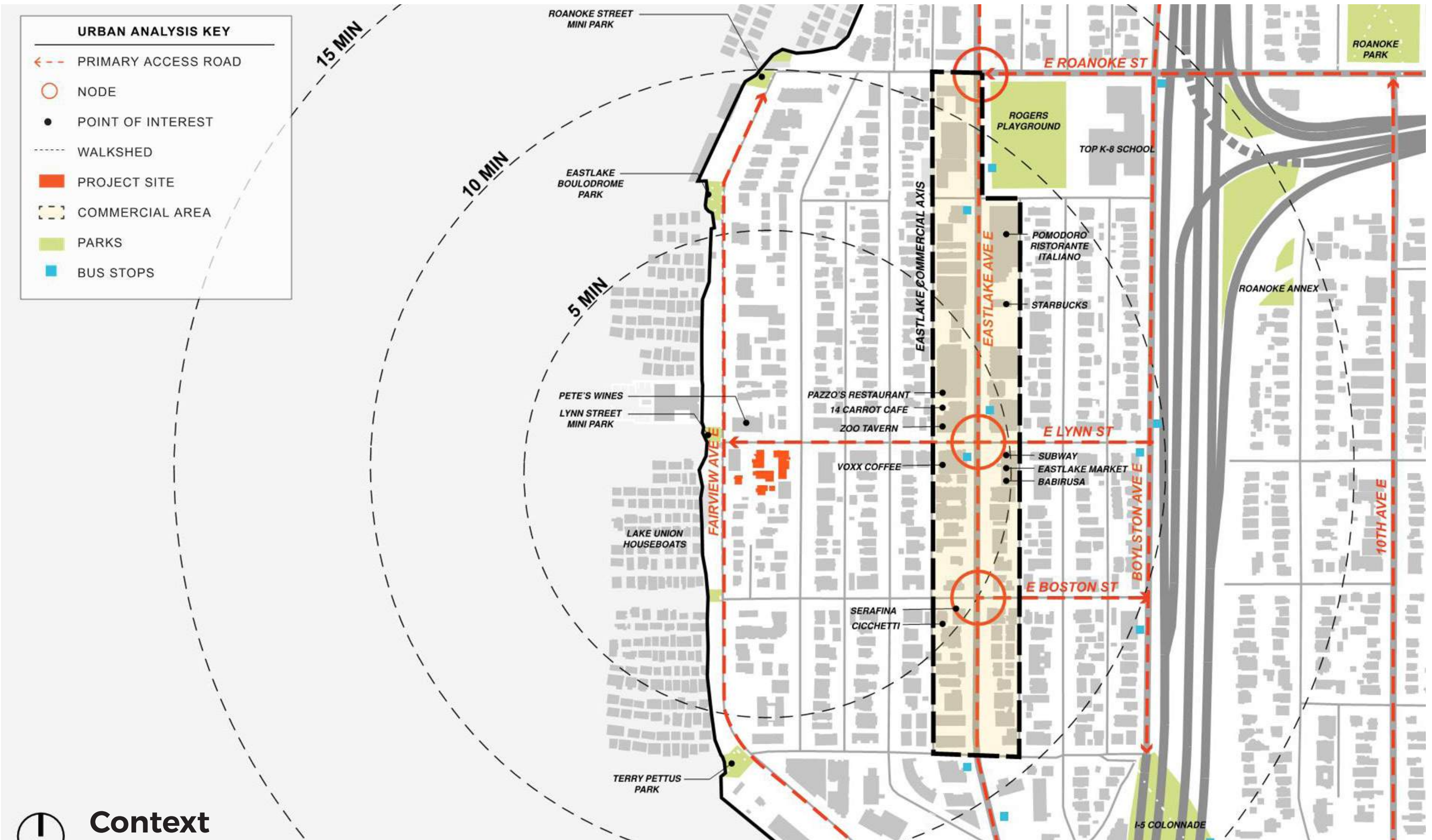


Project Site



Site Context





Context

ADDRESS: 2236 Fairview Ave E
PARCEL #: 290220-0393, 290220-0395, 290220-0400, 290220-1290
ZONING: LR2 / LR2RC
OVERLAYS: Eastlake Residential Urban Village, Frequent Transit
Shoreline Zone - Urban Commercial
ECA: Archaeological Buffer
SITE AREA: 28,042 SF total

23.45.504 PERMITTED USES
Permitted outright: Residential - Rowhouses, Townhomes, Detached Single Family

23.45.514 STRUCTURE HEIGHT
Zoning: LR2
Allowed Maximum Base Height: 30'-0"
3'-0" additional allowed for shed roof: 33'-0"
4'-0" additional allowed for rooftop features (parapets, clerestories, etc.): 34'-0"
5'-0" addition allowed for gable roof: 35'-0"
10' additional allowed for stair penthouses: 40'-0"

23.86.006 STRUCTURE HEIGHT MEASUREMENT
The height of a structure is the difference between the elevation of the highest point of the structure not excepted from applicable height limits and the average grade level ("average grade level" means the average of the elevation of existing lot grades at the midpoint, measured horizontally, of each exterior wall of the structure, or at the midpoint of each side of the smallest rectangle that can be drawn to enclose the structure.)

23.45.510 FLOOR AREA RATIO
Maximum FAR (per parcel):
Rowhouses: 1.1 or 1.3
Townhouses: 1.0 or 1.2

23.45.522 AMENITY AREA
Required: 25% of lot area (SF)
A minimum of 50% is required at ground level (SF)

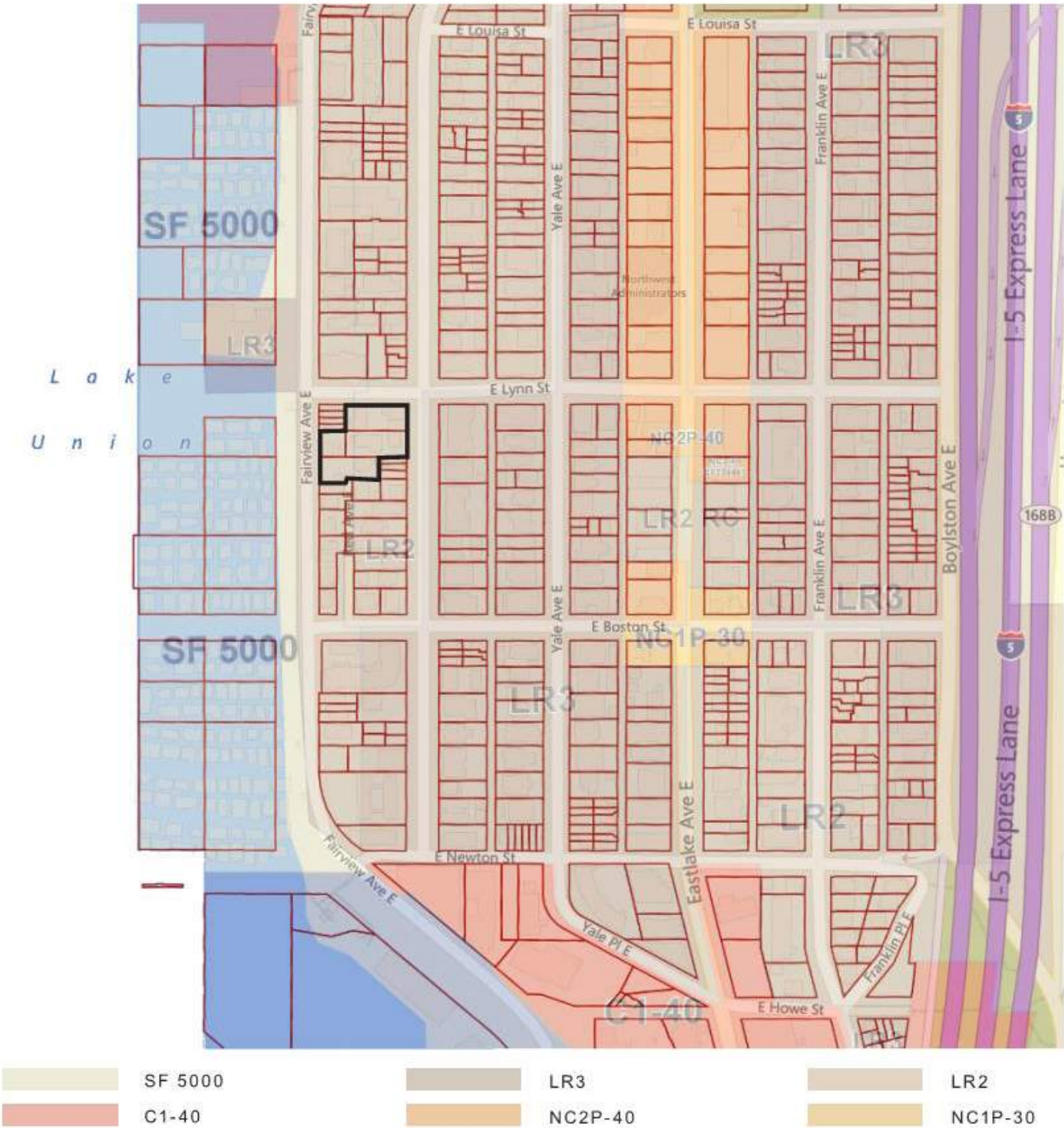
23.45.527 BUILDING WIDTH LIMIT AND MAXIMUM FACADE LENGTH
Structure width:
Rowhouses: No Limit
Townhouses: 90'-0"
Facade length: 65% of lot depth for portions within 15'-0" of a side lot line that is not a street or alley, and 40'-0" for a rowhouse unit located within 15'-0" of a lot line that abuts a single family zone

23.45.524 LANDSCAPING AND SCREENING STANDARDS
• In LR zones, green factor score of .60 or greater, per Section 23.45.524, is required for any lot with development containing more than one new dwelling units.
• Street trees are required when any development is proposed, except as provided in subsection 23.45.524.B.2 and section 23.53.015.
• Existing street trees shall be retained unless the director of transportation approves their removal.
• The Director, in consultation with the director of transportation, will determine the number, type and placement of street trees to be provided.

23.54.015 REQUIRED PARKING
Mapped as Frequent Transit within an Urban Village Overlay: No parking is required.

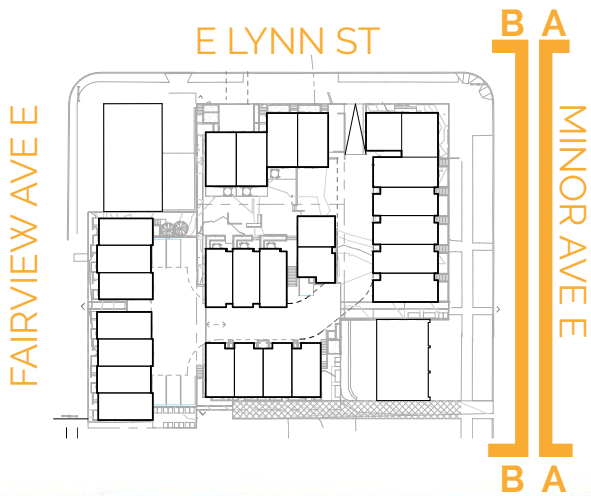
23.54.040 SOLID WASTE & RECYCLABLE MATERIALS STORAGE AND ACCESS
Residential development for which a home ownership association or other entity exists for utility billing may meet the following requirement: 375 square feet minimum shared storage space for a residential development of 26-50 units.

Zoning and Project Information



Street Montages

Existing Street Elevation : Minor Ave E

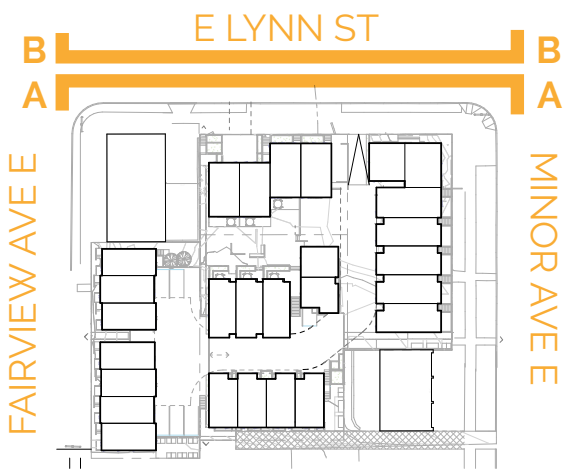


MINOR AVE E LOOKING WEST (B)



MINOR AVE E LOOKING EAST (A)

Street Montages
Existing Street Elevation : E Lynn St



E LYNN ST LOOKING SOUTH (A)

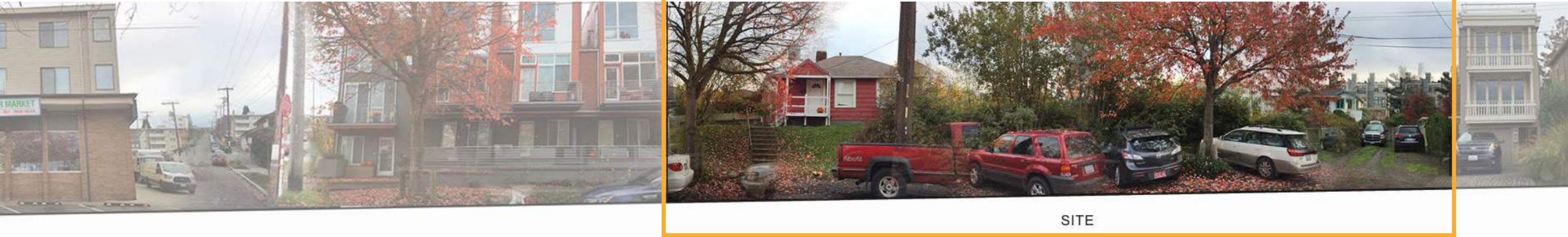
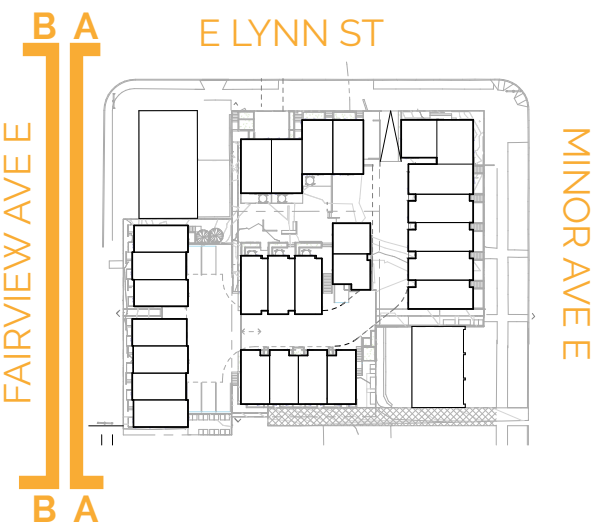


ACROSS FROM SITE

E LYNN ST LOOKING NORTH (B)

Street Montages

Existing Street Elevation : Fairview Ave E

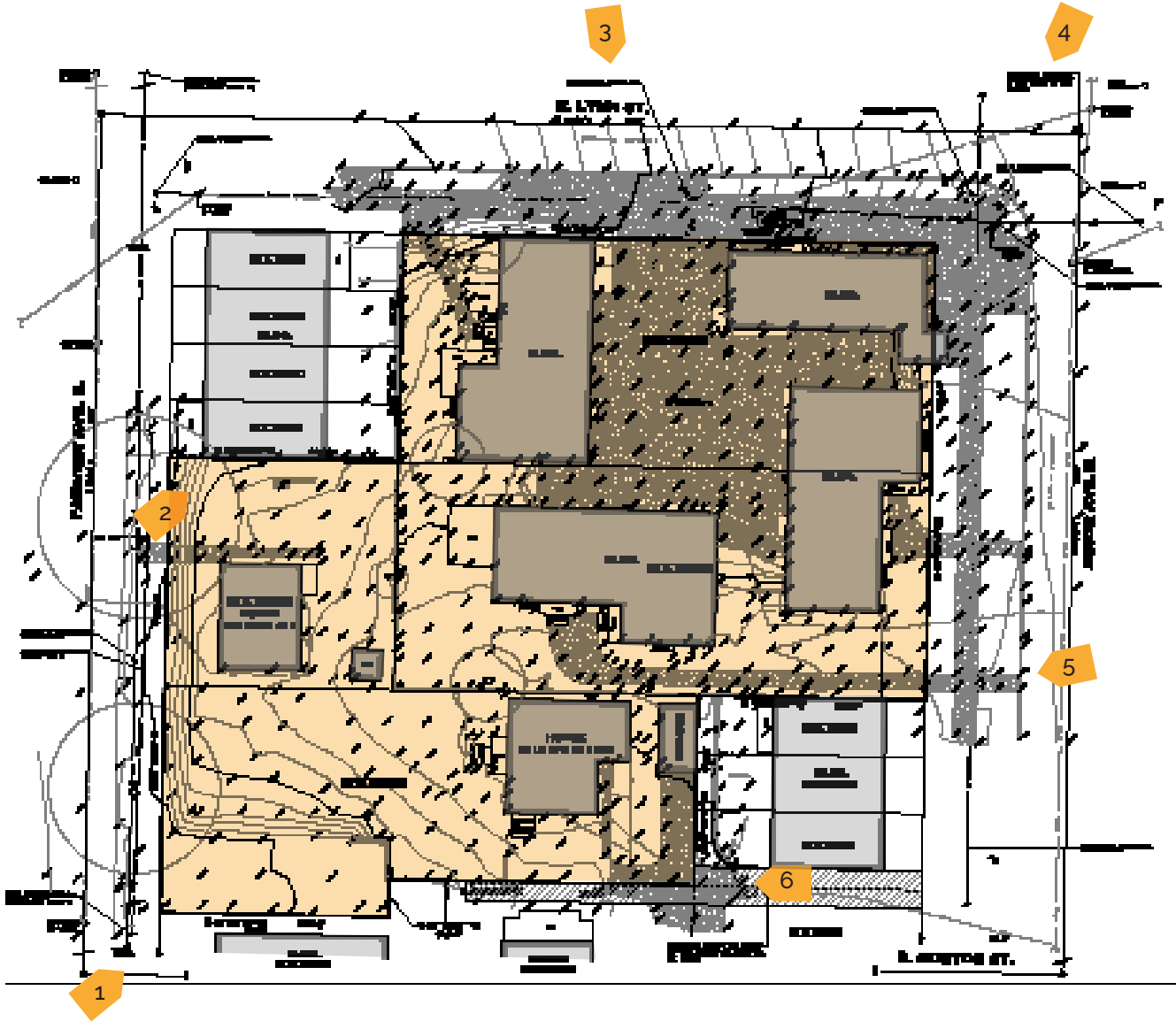


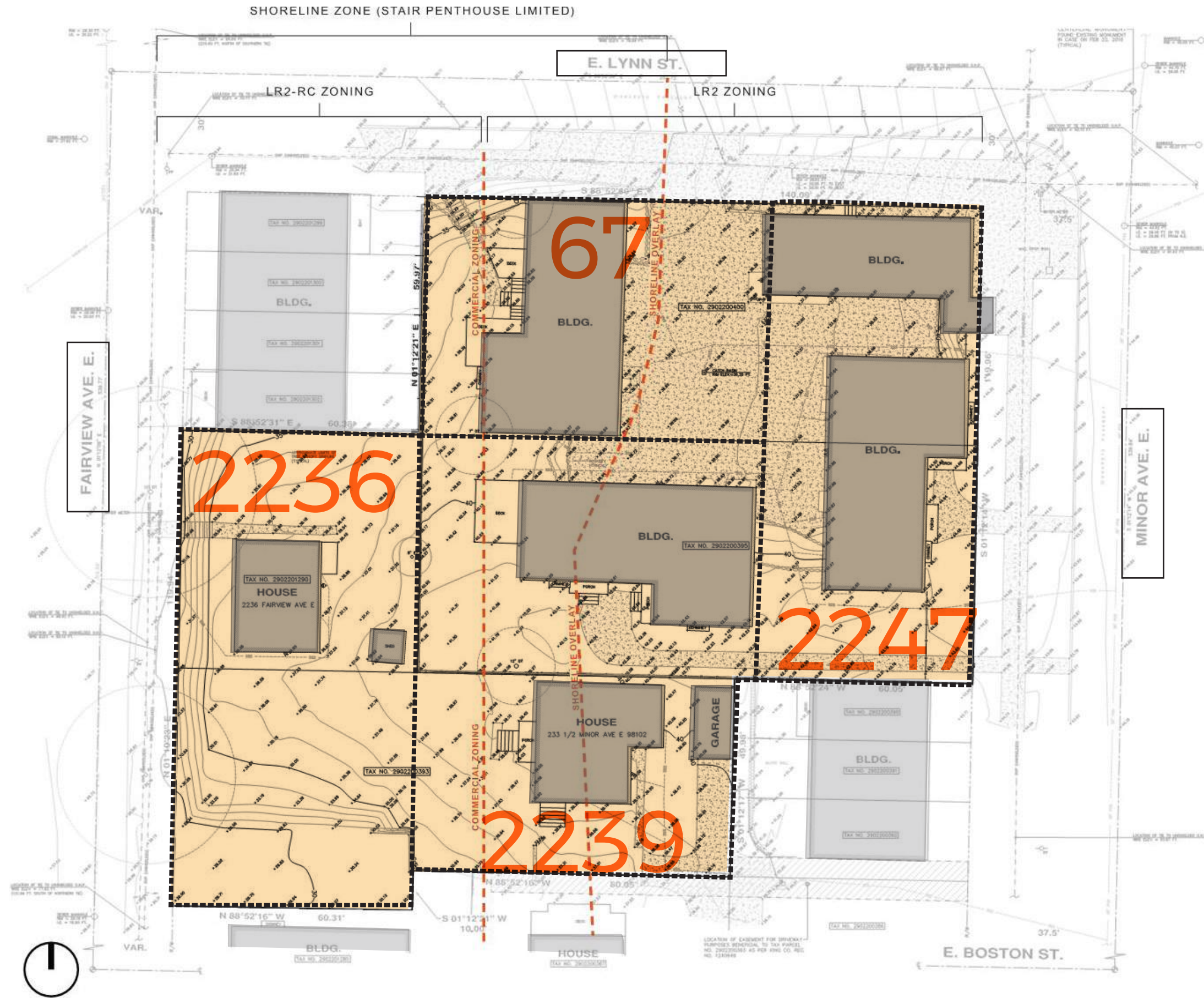
FAIRVIEW AVE E LOOKING EAST (A)



FAIRVIEW AVE E LOOKING WEST (B)

Context Images





EXISTING SITE CONDITIONS

PROPOSED PROJECT SITE

Four parcels bordered by Fairview Ave E, E Lynn St, and Minor Ave E
Site area: 28,042 SF total

TOPOGRAPHY

Approx. 14'-0" slope down across site from east to west

ADJACENT BUILDINGS AND USES

- Existing 3-story rowhouses (4) to the northwest of site
- Existing 3-story rowhouses (3) to the southeast of site
- Existing 3-story single-family residence to the southwest of site
- Existing 3-story single-family residence to the direct south of site

SOLAR ACCESS & VIEWS

The site has good solar access due to existing sloped topography, adjacency to streets, and proximity to Lake Union to the west
Site will generally have views towards the west (Lake Union) and towards the south (Downtown Seattle).

TRAFFIC CIRCULATION

Fairview Ave E is a heavy pedestrian street; proposed site traffic will not be allowed to discharge onto this street.
There are no traffic signals in the immediate site, but there is a 4-way stop sign at Lynn and Fairview and yield signs for N/S traffic on Minor at Lynn to facilitate traffic patterns.
There is regulated street parking along the three adjacent streets.
There are no bus stops at the immediate adjacent streets; however, there are several two blocks east along Eastlake Ave E.

STREETSCAPE

Minor Ave E:
Sidewalk: Approx. 6'-0" wide
Planting Strip: Approx. 11'-0" wide
Street Trees: Yes
Parking: Yes, 2 hour limit or RPZ Zone 8

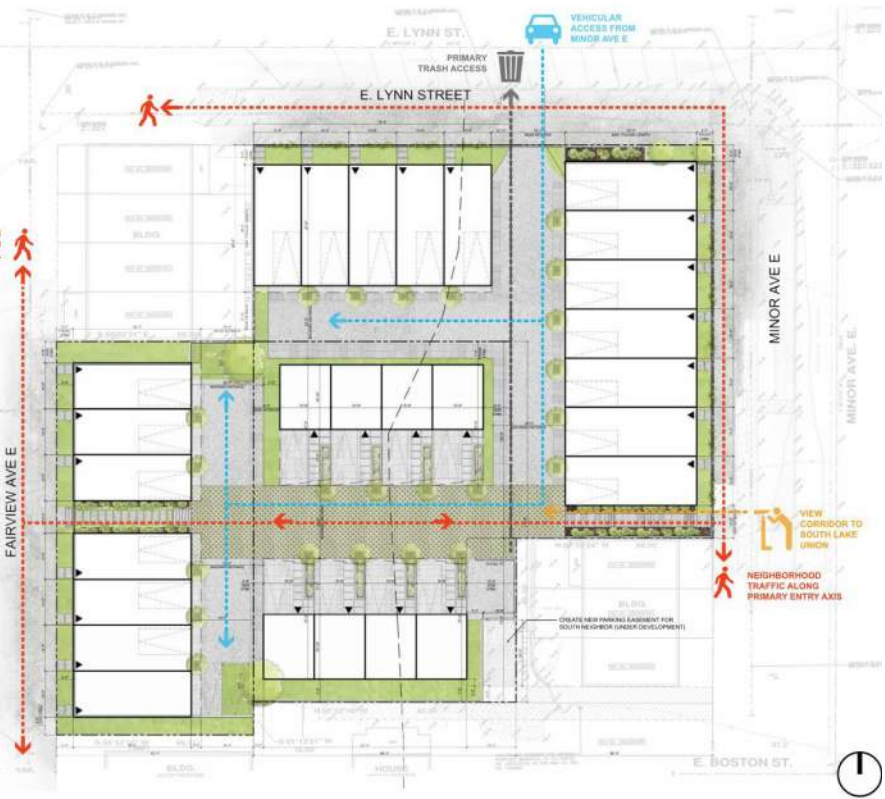
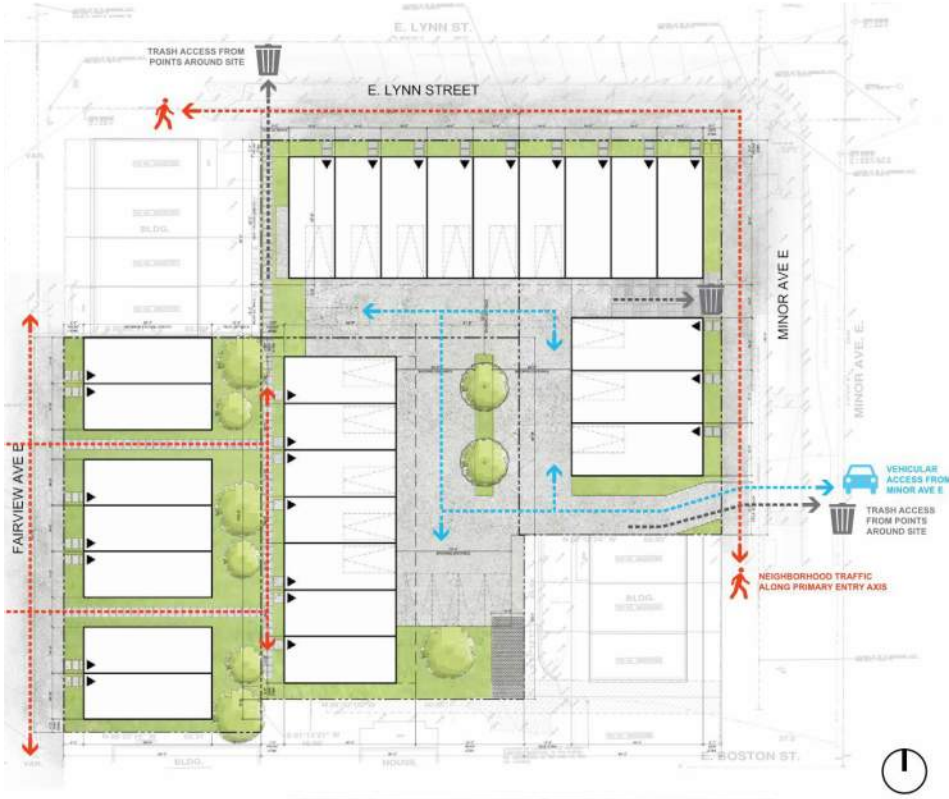
E Lynn St:
Sidewalk: Approx. 7'-0" wide
Planting Strip: Approx. 5'-0" wide
Street Trees: Yes
Parking: Yes, 2 hour limit or RPZ Zone 8

Fairview Ave E:
Sidewalk: None
Planting Strip: None
Street Trees: Yes
Parking: Yes, unrestricted parallel parking

SHORELINE ZONE

Approximately half of the site is located in the Urban Commercial Shoreline Zone.

EDG SUMMARY



EDG Option 2 - WOONERF

27 units, 16 parking spots
units 13-14.5ft wide - Fairview & Lynn
units 15-15.5ft wide - Minor & Interior
(4) stories w/ increased excavation
More pavement and garage doors



EDG Option 3 - VIEW CORRIDOR

27 units, 27 parking spots
units 13-14.5ft wide - Fairview & Lynn
units 15-15.5ft wide - Minor & Interior
(4) stories w/ increased excavation
More pavement and garage doors



Recommendation

27 units, 27 parking spots
units 16-17.5 ft wide - Fairview & Lynn
units 16-17.5ft wide - Minor & Interior
(4) stories w/ minimized excavation
reduced paving and mix of garage & surface parking

EDG GUIDANCE AND RESPONSE

The board discussed the merits of massing Option Two and Three to establish a strong sense of community and integrate thoughtfully with the neighborhood, creating an open space on site with a courtyard and woonerf and by maintaining the view corridor across the site.

PUBLIC COMMENT

The following public comments were offered at this meeting:

- Supported creating a view corridor through the site by breaking up the massing of the units along Minor Avenue East.
- Supported the woonerf concept and the idea of adding some color to the project.
- Concerned with the affordability of the units.
- Supported the use of articulation to break up the façade along all street frontages.
- Concerned with the contrast of the proposed architectural style with that of the existing neighborhood, the lack of street engagement throughout the project, and the uninviting entries to the units.
- Concerned with the monolithic nature of the units. Would support the introduction of some variety in the units and façades.
- Supported incorporating units with flat roofs, open staircases, and open railing along Minor Avenue East.
- Concerned with the centralizing of the garbage.
- Concerned with the lack of design features on the units facing the street.
- Supported enhancing the sidewalk.

SDCI staff also summarized design related comments received in writing prior to the meeting:

- Neighbor whom shares driveway easement requested to be made a party of record.
- Concerned with the impact of views to Lake Union.
- Opposed the use of gable roofs for the townhouses on site.

PRIORITIES & BOARD RECOMMENDATIONS

1. Massing and Architectural Concept:

The Board discussed the merits of massing Options Two and Three, analyzing the open space on site and how the massing typology of the residential units fit into the surrounding neighborhood character. The Board believed the layout configuration and inclusion of the woonerf in Option Two provided the best opportunity to create a successful open space on site, as well as, help to establish a sense of

community for the project. Also discussed in detail was the view corridor found in Option Three and how it integrated with the neighborhood and pedestrians traveling along Fairview Avenue East. After a thoughtful discussion, the Board supported moving forward with development of Option Two, with adherence to the following guidance:

- a. Acknowledging public comment, the Board discussed the use of the gable roof form as a mechanism for reducing the perceived massing of the structures and as a method for establishing character for the individual units throughout the site. The Board recommended the applicant explore areas where the gable roof can be most successful and areas where the flat roof form is more appropriate in terms of providing views to Lake Union and adding some variety to the proposal. (CS2-D Height, Bulk, and Scale)
- b. E Lynn St. The Board echoed public comment and strongly recommended incorporating more variation (changing rooflines, façade modulation, etc.) in the townhouses located along E Lynn Street to break up the monotony of the massing. The Board acknowledged the topography created some variety due to the slope, however, commented that greater variety was needed. The Board suggested exploring the following with the goal of creating a sense of individuality, identity, and character (CS1-C Topography, CS2-B Adjacent Sites, Streets, and Open Spaces, CS2- C-3 Full Block Sites, DC1-A-2 Gathering Places):
 - i. The Board recommended locating some, or all, of the roof decks for the townhouses along E Lynn Street and Minor Avenue East toward the street to create variety in massing modulation. (CS2-B-2 Connection to the Street, PL2- B Safety and Security)
 - ii. The Board strongly recommended incorporating the use of color in the material palette to help establish a sense of character for the proposal. (CS3-A Emphasizing Positive Neighborhood Attributes)
 - iii. The Board suggested the applicant explore areas where secondary architectural features can be utilized to break up the massing and provide some detail to the townhouses. (DC2-A-2 Reducing Perceived Mass , DC2-C Secondary Architectural Features)
 - iv. The Board referenced the precedent image shown on page 39 of the EDG packet which used subtle variety in the composition of windows and modulation along with color to create variation and identity for each unit. (DC2-B-1. Façade Composition)

- c. Fairview Ave. The Board was supportive of the 2-3-2 building grouping as this created a successful residential scale and rhythm. (CS3-A-4 Evolving Neighborhoods)

2. Residential Entries:

- a. The Board expressed their concerns with the parking/entry sequence for the interior units of the proposal. At the next meeting, the Board would like clarification of how the entries for the interior units will be treated. (PL3-A Entries, PL3-B Residential Edges)
- b. The concept of the sunken patios adjacent to entries along the street frontages intrigued the Board, however, the Board expressed concern for the fully sunken condition and was more comfortable with the partially sunken patio condition. At Recommendation, the Board would like to see more details related to this condition. (PL3-A Entries, PL3-B Residential Edges)
- c. The Board agreed that the use of fencing around the project should not be used as a method for providing security to the units. The Board recommended exploring the use of transitional spaces to help further distinguish the entry areas and to provide a buffer from public to private, while staying engaged with the public realm. (PL3-B-1 Security and Privacy)
- d. The Board acknowledged public concern about interacting with the street and sidewalk and strongly supported the use of stoops along the street frontage. This is especially important for the units that are easily accessible from the sidewalk. (PL3-A Entries, PL3-B Residential Edges, DC2-D-1 Human Scale)

3. Open Space and Street Engagement:

- a. The Board agreed with public comment and expressed strong support for the woonerf concept, highlighting its potential to create a successful communal space that would be utilized by the residents. (PL1-A Network of Open Spaces, DC1-A-2 Gathering Places , DC1-C-3 Multiple Uses, DC4-D Trees, Landscape, and Hardscape Materials)
- b. The Board agreed with public comment and encouraged the applicant to explore incorporating the view corridor present in Option Three into the design of Option Two and possibly connecting it to the proposed woonerf. This would involve providing a break in the layout of the central

units. The Board expressed they would be open to a departure on Parcel C if this would assist in accommodating the view corridor and result in a stronger tie to the neighborhood. (CS2-B Adjacent Sites, Streets, and Open Spaces, PL1-B Walkways and Connections, PL1-C-1 Selecting Activity Areas)

- c. The Board recommended pushing the units facing Fairview Avenue East further back from the street maximizing greenspace and vegetation along the designated neighborhood greenway. (PL1-A-1 Enhancing Open Space)

4. Recycling/Waste Enclosures:

Hearing public concern, the Board encouraged exploration of multiple trash enclosure locations versus one location as presented at EDG. The Board strongly encouraged accommodated internal collection per the recommendation of SDOT. The Board also suggested the applicant investigate the impacts of having a singular trash enclosure on neighboring properties. (DC1-C-4 Service Uses)

EDG GUIDANCE:

- 1 Massing and Architectural Concept
- 2 Residential Entries
- 3 Open Space and Street Engagement
- 4 Recycling and Waste Enclosures



MASSING AND ARCHITECTURAL CONCEPT 1

The board believed the **woonerf** in Option Two provided the best opportunity to create a **successful open space** on site to **establish a sense of community**. The **view corridor** in Option Three is also incorporated to **integrate with the surrounding neighborhood**.

EDG DESIGN GUIDANCE:

1. Massing & Design Concept:

The Board discussed the merits of massing Options Two and Three, analyzing the open space on site and how the massing typology of the residential units fit into the surrounding neighborhood character. The Board believed the layout configuration and inclusion of the woonerf in Option Two provided the best

opportunity to create a successful open space on site, as well as, help to establish a sense of community for the project. Also discussed in detail was the view corridor found in Option Three and how it integrated with the neighborhood and pedestrians traveling along Fairview Avenue East. After a thoughtful discussion, the Board supported moving forward with development of Option Two.

RESPONSE:

Project combines the overall strengths of massing option two and massing option three to promote a sense of community and respect for adjacent sites in two dynamic ways:

First, the woonerf concept from Option 2 was maintained and developed, increasing the usability of the community active space while still allowing for vehicles to park within garage spaces at a lower level.

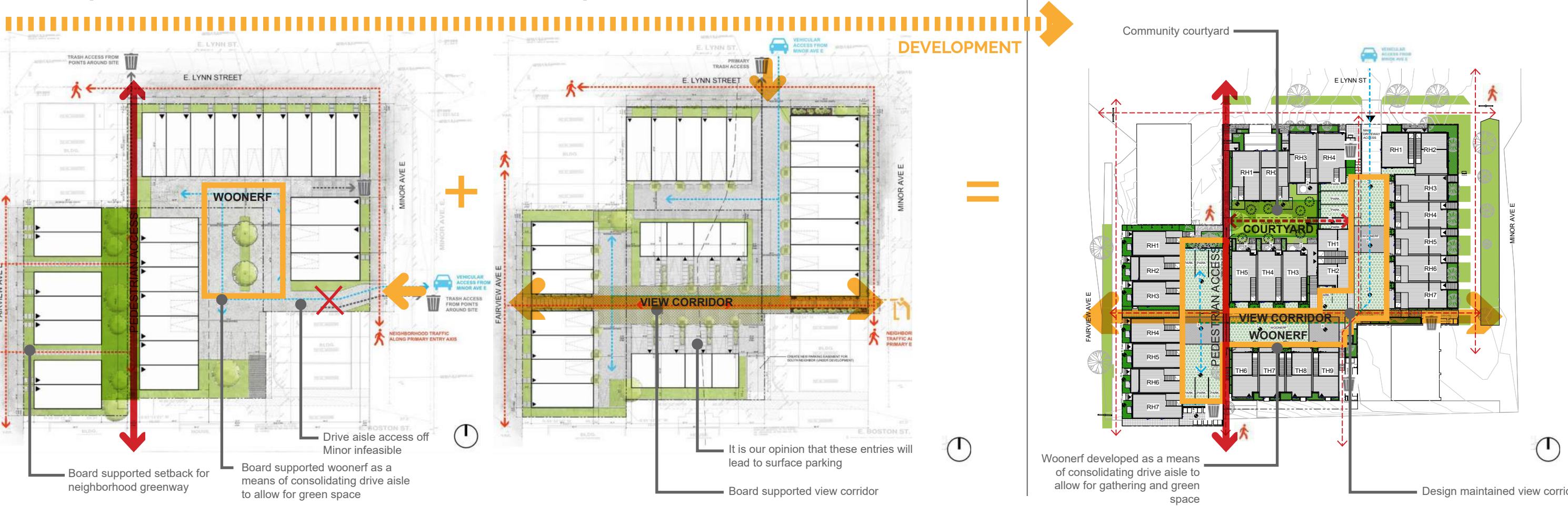
Secondly, the view corridor(Option 3) from west to east was maintained through the site to provide a visual (and physical) connection across the development.

A secondary pedestrian access from north to south was also maintained from Option 2 to more closely knit the neighborhood together. A green courtyard is located off this route.

EDG Option 2 - WOONERF

EDG Option 3 - VIEW CORRIDOR

Recommendation





Site Plan

Project Development

- 1 Maintain Woonerf Concept
- 2 Green Courtyard
- 3 Maintain View Corridor
- 4 Develop Pedestrian Connections

EDG DESIGN GUIDANCE:
1A Massing and Design Concept:

a. Acknowledging public comment, the Board discussed the use of the gable roof form as a mechanism for reducing the perceived massing of the structures and as a method for establishing character for the individual units throughout the site. The Board recommended the applicant explore areas where the gable roof can be most successful and areas where the flat roof form is more appropriate in terms of providing views to Lake Union and adding some variety to the proposal. (CS2-D Height, Bulk, and

RESPONSE:

Flat roofs were incorporated throughout the development to respond to the public comment to reduce the bulk and scale of the massing. Views from the east towards Lake Union are preserved by not providing additional height through roof penthouses or additional height through gabled roof forms. Additionally, open railings have been incorporated throughout the development to break down the scale and add texture and variety to the street edges.

Public comments supported flat roofs, open stairs to the top level and further articulation to break up street frontages and to enhance the sidewalk and pedestrian experience.

EDG DESIGN GUIDANCE:

1A Massing and Design Concept:

a. Acknowledging public comment, the Board discussed the use of the gable roof form as a mechanism for reducing the perceived massing of the structures and as a method for establishing character for the individual units throughout the site. The Board recommended the applicant explore areas where the gable roof can be most successful and areas where the flat roof form is more appropriate in terms of providing views to Lake Union and adding some variety to the proposal. (CS2-D Height, Bulk, and

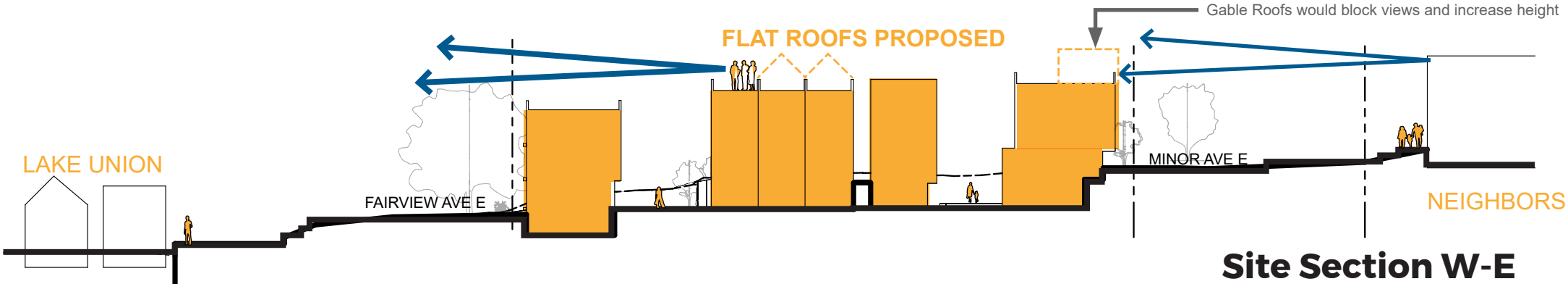
RESPONSE:

Flat roofs were incorporated throughout the development to respond to the public comment to reduce the bulk and scale of the massing. Views from the east towards Lake Union are preserved by not providing additional height through roof penthouses or additional height through gabled roof forms. Additionally, open railings have been incorporated throughout the development to break down the scale and add texture and variety to the street edges.

- protect views
- reduce bulk, scale and height
- provide open space opportunities



Recommendation:



E Lynn Street Development

EDG DESIGN GUIDANCE:

1B - E Lynn Street

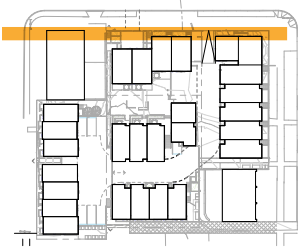
b. E Lynn St. The Board echoed public comment and strongly recommended incorporating more variation (changing rooflines, façade modulation, etc.) in the townhouses located along E Lynn Street to break up the monotony of the massing. The Board acknowledged the topography created some variety due to the slope, however, commented that greater variety was needed. The Board suggested exploring the following with the goal of creating a sense of individuality, identity, and character (CS1-C Topography, CS2-B Adjacent Sites, Streets, and Open Spaces, CS2- C-3 Full Block Sites, DC1-A-2 Gathering Places):

i. The Board recommended locating some, or all, of the roof decks for the townhouses along E Lynn Street and Minor Avenue East toward the street to create variety in massing modulation. (CS2-B-2 Connection to the Street, PL2- B Safety and Security)

ii. The Board strongly recommended incorporating the use of color in the material palette to help establish a sense of character for the proposal. (CS3-A Emphasizing Positive Neighborhood Attributes)

iii. The Board suggested the applicant explore areas where secondary architectural features can be utilized to break up the massing and provide some detail to the townhouses. (DC2-A-2 Reducing Perceived Mass , DC2-C Secondary Architectural Features)

iv. The Board referenced the precedent image shown on page 39 of the EDG packet which used subtle variety in the composition of windows and modulation along with color to create variation and identity for each unit. (DC2-B-1. Façade Composition) Scale)



EDG VIEW
E Lynn Elevation

RESPONSE:

Variation in the massing was incorporated by breaking up the monotony of the building into three parts, differentiated through a change in the color of the brick, a high-quality element that will contribute to a more residential feel. Additional architectural features, including recessed stoops, fenestration patterns and open guardrails also help to add character and break up the perceived mass of the structure.



RECOMMENDATION
E Lynn Elevation

E Lynn Street Development



Lynn St Rowhouse respect the elegant brick apartments apartments throughout the neighborhood.

b. E Lynn St. The Board echoed public comment and strongly recommended incorporating more variation (changing rooflines, façade modulation, etc.) in the townhouses located along E Lynn Street to break up the monotony of the massing. The Board acknowledged the topography created some variety due to the slope, however, commented that greater variety was needed. The Board suggested exploring the following with the goal of creating a sense of individuality, identity, and character (CS1-C Topography, CS2-B Adjacent Sites, Streets, and Open Spaces, CS2- C-3 Full Block Sites, DC1-A-2 Gathering Places):

i. The Board recommended locating some, or all, of the roof decks for the townhouses along E Lynn Street and Minor Avenue East toward the street to create variety in massing modulation. (CS2-B-2 Connection to the Street, PL2- B Safety and Security)

ii. The Board strongly recommended incorporating the use of color in the material palette to help establish a sense of character for the proposal. (CS3-A Emphasizing Positive Neighborhood Attributes)

iii. The Board suggested the applicant explore areas where secondary architectural features can be utilized to break up the massing and provide some detail to the townhouses. (DC2-A-2 Reducing Perceived Mass , DC2-C Secondary Architectural Features)

iv. The Board referenced the precedent image shown on page 39 of the EDG packet which used subtle variety in the composition of windows and modulation along with color to create variation and identity for each unit. (DC2-B-1. Façade Composition)



i. Staggered Roof Decks along street



ii. Color to add character



iii. Secondary architectural elements



iv. Modulation to create identity for each unit.

Fairview Avenue Development

EDG DESIGN GUIDANCE:

1C - Fairview Avenue

c. Fairview Ave. The Board was supportive of the 2-3-2 building grouping as this created a successful residential scale and rhythm. (CS3-A-4 Evolving Neighborhoods)

RESPONSE:

Building grouping has been developed to maintain the view corridor recommended within the design guidance from Option 3 (EDG). While slightly modified to maintain a more direct pedestrian connection through the site, the massing has been pushed and pulled to add texture, variety and street articulation. Additionally, material variety, raised entry stoops and secondary architectural features such as the open railing mixed throughout the elevation help break down the overall scale of the structure to compliment the residential street of Fairview Avenue.

Fairview Ave Rowhouses have a direct relationship with the waterfront and a dock house vibe.

Inspiration Imagery



EDG VIEW
Fairview Elevation



RECOMMENDATION
Fairview Avenue Elevation

Fairview Avenue Development



Concept image for accent Panels



1. Dark Board and Batten



2. White Board and Batten



3. Black Window Trim and White Accent Panels



4. Pop of warmth at entry

Minor Ave E Development

RESPONSE AND DEVELOPMENT:

The general building grouping along minor has been maintained, 4-2. The design has been elevated to better turn the corner at E. Lynn St and covered unit entries have been raised to better respond to topography and provide some seperation from the sidewalk along Minor Ave E. The material differentiation adds variety and gives individual identity to the units along this particular street.

EDG VIEW
Minor Ave E Elevation



RECOMMENDATION
Minor Ave E Elevation

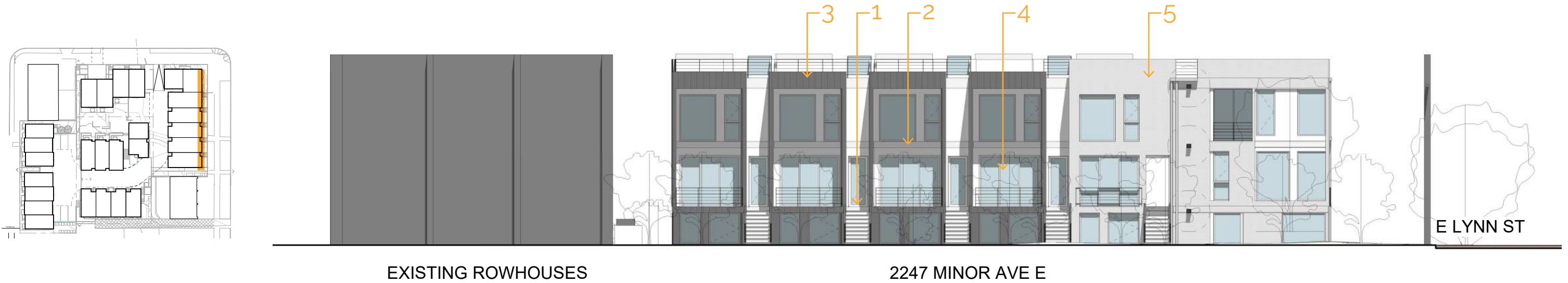
Minor Ave E Development



Minor Ave Rowhouses express their individuality and engage the sidewalk through dense planting and large stoops.



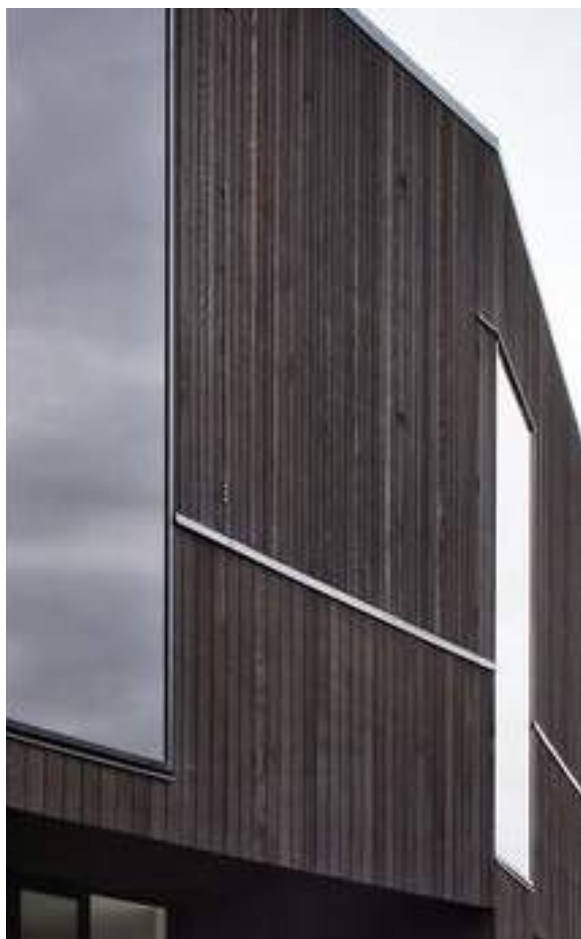
Minor Ave E Development



1. Open Tread Stairs



2. White hardi panel



3. Gray stained cedar



4. Dark Window Trim / Open Railings



5. Lynn Street Facade turns corner



RESIDENTIAL ENTRIES 2

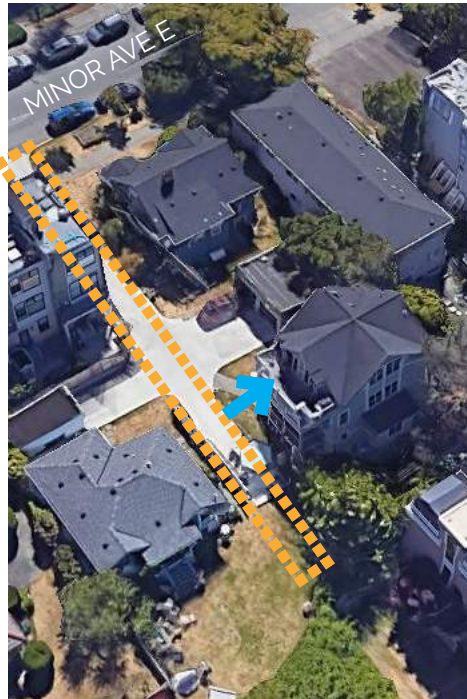
The board expressed concern with the parking / entry sequence for the interior units of the proposal and requested clarification on how residential entries will be treated throughout the development.

EDG DESIGN GUIDANCE:
2: Residential Entries:

- a. The Board expressed their concerns with the parking/entry sequence for the interior units of the proposal. At the next meeting, the Board would like clarification of how the entries for the interior units will be treated. (PL3-A Entries, PL3-B Residential Edges)
- b. The concept of the sunken patios adjacent to entries along the street frontages intrigued the Board, however, the Board expressed concern for the fully sunken condition and was more comfortable with the partially sunken patio condition. At Recommendation, the Board would like to see more details related to this condition. (PL3-A Entries, PL3-B Residential Edges)

RESPONSE:

Most units along the street-facing facades have raised entry stoops to act as a transitional zone off of the sidewalk. Additionally, landscape and bio-retention basins provide an additional layer of seperation from public to private space. The interior unit entries have been re-configured either off a new green courtyard space on the north or along a shared access easement for the southern units to allow for seperation from the predominant vehicular access woonerf through the middle of all the units. Most back doors and garage level access is off the woonerf allowing front doors to be off pedestrian sidewalks, green spaces or shared easements. All sunken patios were also eliminated in lieu of gaining more usable amenity space.



AERIAL OF ACCESS EASEMENT



Sense of Character

The Board strongly recommended incorporating the use of color in the material palette to help establish a sense of character for the proposal.



Stairs to Entry Doors



Warm Entry Tones



Wood Deck Accents



Color Signage / Wayfinding

2. Residential Entries:

b. The concept of the sunken patios adjacent to entries along the street frontages intrigued the Board, however, the Board expressed concern for the fully sunken condition and was more comfortable with the partially sunken patio condition. At Recommendation, the Board would like to see more details related to this condition. (PL3-A Entries, PL3-B Residential Edges)



Stooped entries



Accent Color



Entry seating



Lighting and addressing signage

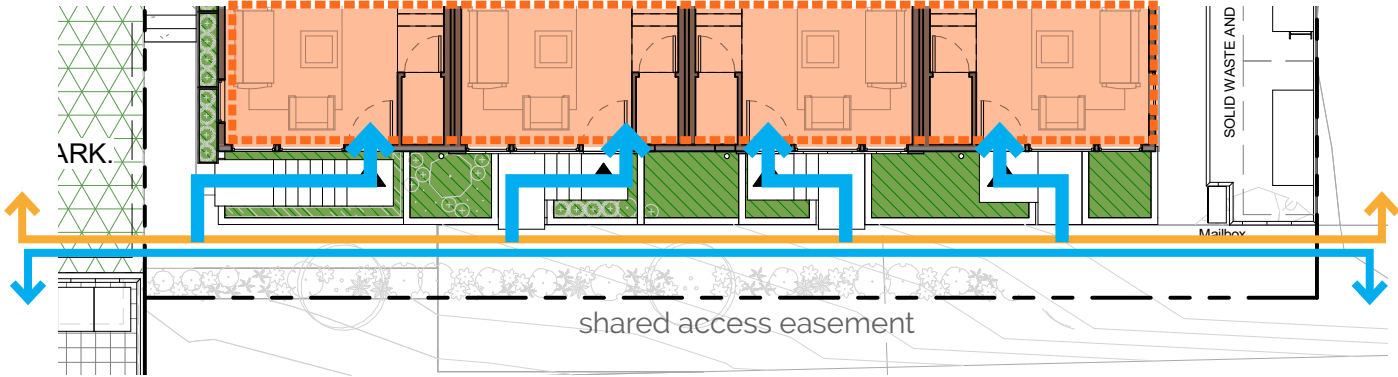
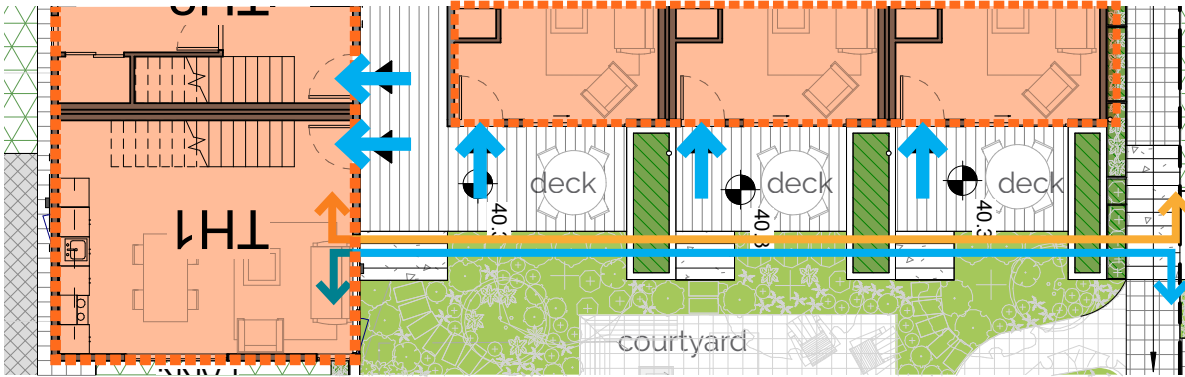
Interior Unit Entries

Elevation



Plan

PRIVATE
SEMI-PUBLIC
PUBLIC



➔
INTERIOR
UNIT ENTRY



RESPONSE:

Interior unit entries have been developed to be distinct and intimate in scale with the mass above cantilivering out to provide weather protection for Seattle's rainy climate. Additionally, thought has been given to provide a transitional zone (semi-public) space between the more public sidewalks and courtyard spaces and the private space within the units. These transitional zones provide a sense of welcome and arrival through the design of deck spaces, small patios, plantings and raised entry stoops. Signage will also be incorporated, both to navigate through the overall development and to signal the individual unit address.

< INTERIOR UNIT ENTRY OFF COURTYARD

Street-facing Unit Entries

EDG DESIGN GUIDANCE:

- c. The Board agreed that the use of fencing around the project should not be used as a method for providing security to the units. The Board recommended exploring the use of transitional spaces to help further distinguish the entry areas and to provide a buffer from public to private, while staying engaged with the public realm. (PL3-B-1 Security and Privacy)
- d. The Board acknowledged public concern about interacting with the street and sidewalk and strongly supported the use of stoops along the street frontage. This is especially important for the units that are easily accessible from the sidewalk. (PL3-A Entries, PL3-B Residential Edges, DC2-D-1 Human Scale)

RESPONSE:

The developed design has been enhanced through landscaping and planting buffers as a tool to eliminate fences and gates through the design. Additionally, transitional zones, such as courtyards and the woonerf have been designed in such a way as to promote active amenity space and as an area for community interaction and gathering. Raised entry stoops along the street and sidewalk edge have been incorporated wherever grade allows in order to create a sense of entry, maximize privacy in the units and provide eyes on the street for safety.

Elevations

E. LYNN ST



FAIRVIEW AVE



E. LYNN / MINOR AVE E



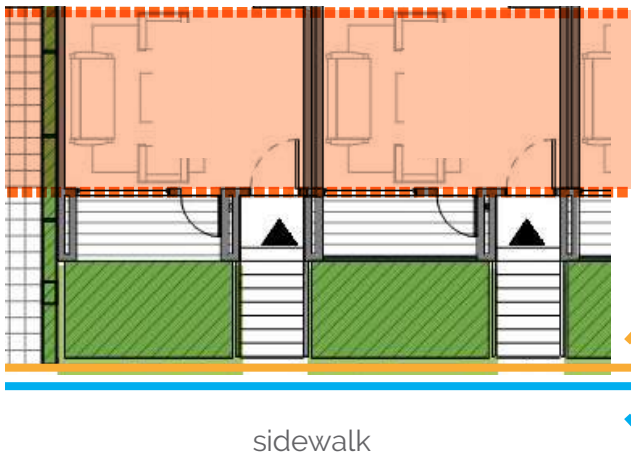
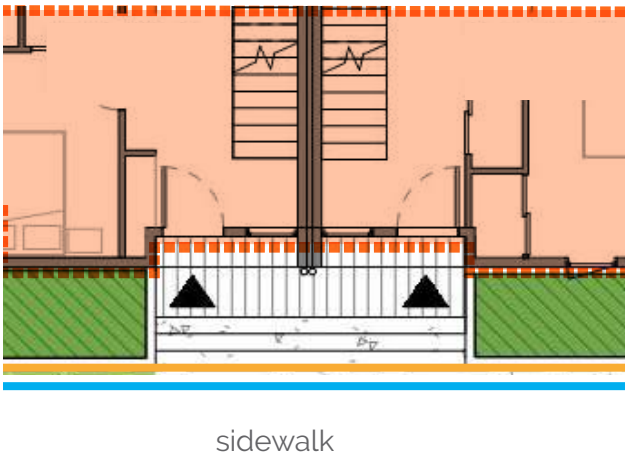
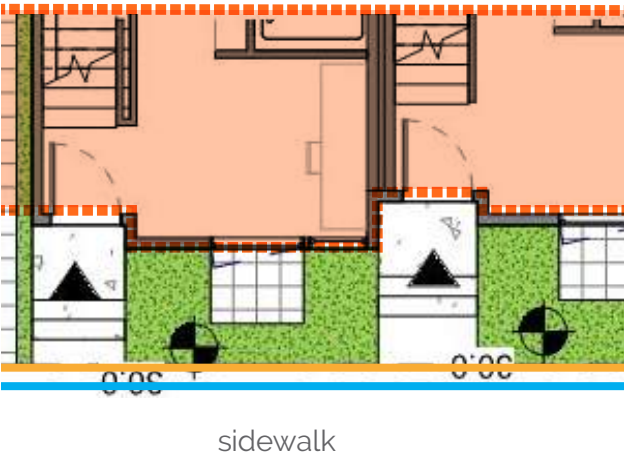
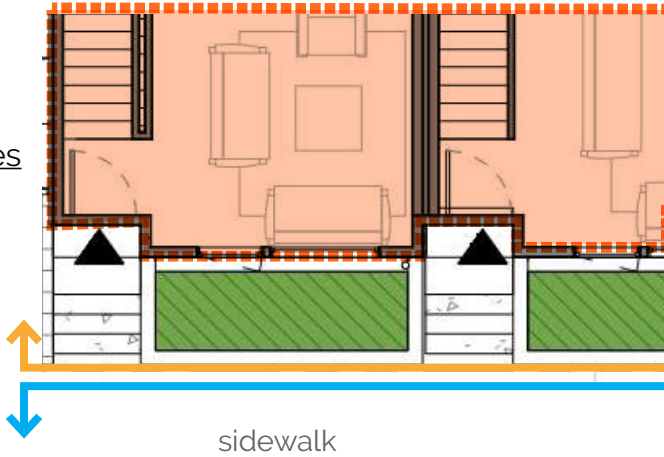
MINOR AVE E



Plans

Creating Transition Zones

- PRIVATE
- SEMI-PUBLIC
- PUBLIC





The board supported a **woonerf as an opportunity to create successful open space and encouraged **incorporating the view corridor** to tie into the neighborhood.**

EDG GUIDANCE:

3. Open Space and Street Engagement:

a. The Board agreed with public comment and expressed strong support for the woonerf concept, highlighting its potential to create a successful communal space that would be utilized by the residents. (PL1-A Network of Open Spaces, DC1-A-2 Gathering Places, DC1-C-3 Multiple Uses, DC4-D Trees, Landscape, and Hardscape Materials)

b. The Board agreed with public comment and encouraged the applicant to explore incorporating the view corridor present in Option Three into the design of Option Two and possibly connecting it to the proposed woonerf. This would involve providing a break in the layout of the central units. The Board expressed they would be open to a departure on Parcel C if this would assist in accommodating the view corridor and result in a stronger tie to the neighborhood. (CS2-B Adjacent Sites, Streets, and Open Spaces, PL1-B Walkways and Connections, PL1-C-1 Selecting Activity Areas)

RESPONSE:

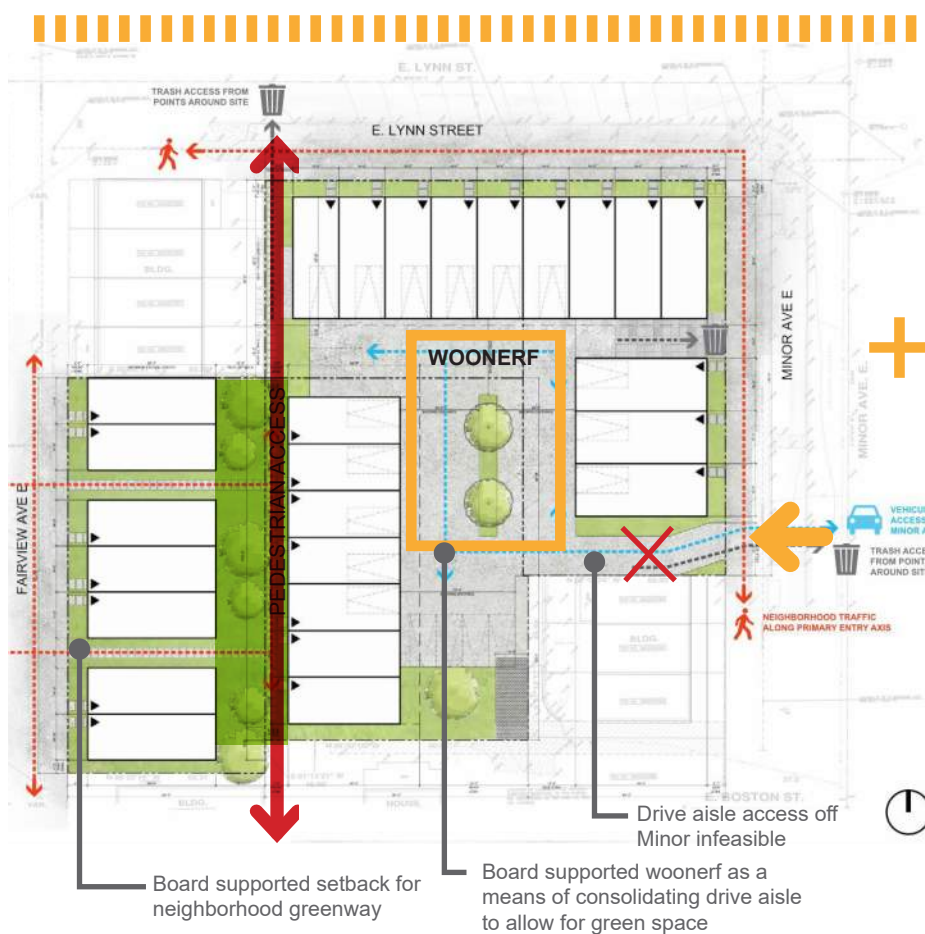
a. Project combines the overall strengths of massing option two and massing option three to promote a sense of community and respect for adjacent sites in two dynamic ways:

First, the woonerf concept from Option 2 was maintained and developed, increasing the usability of the community active space while still allowing for vehicles to park within garage spaces at a lower level.

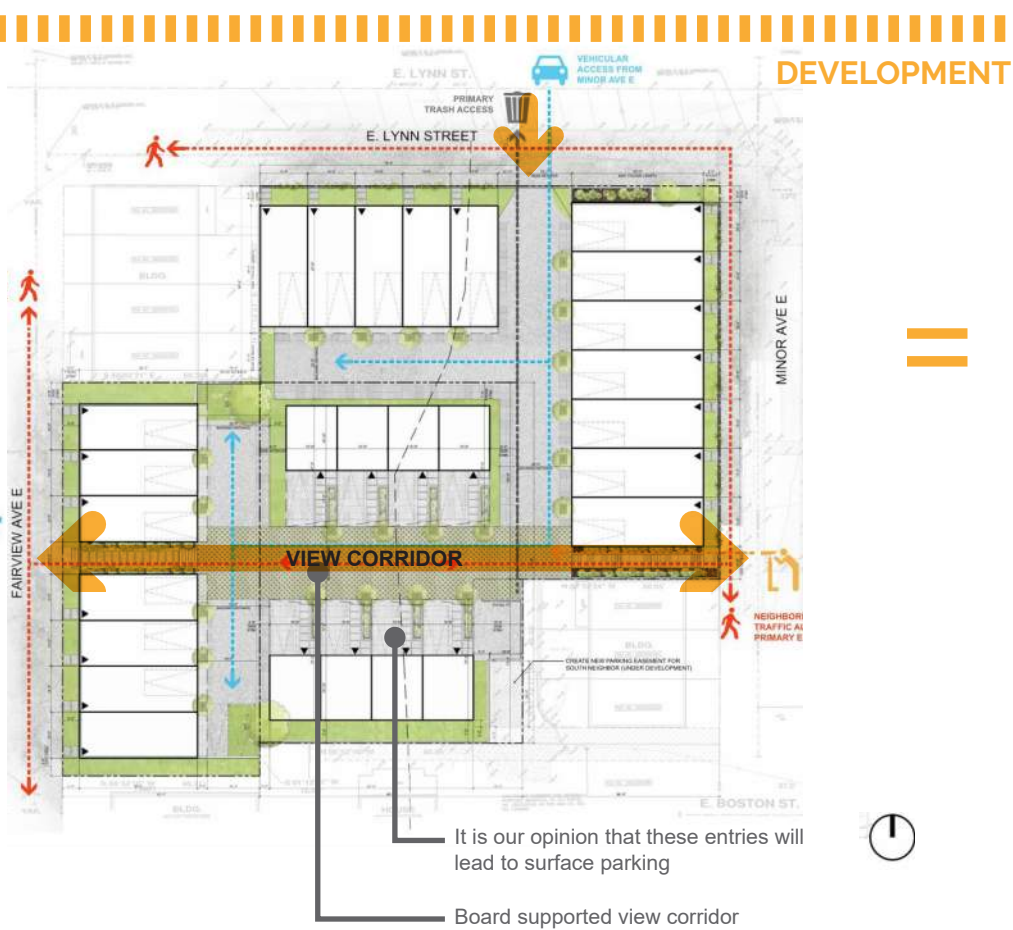
b. Secondly, the view corridor (Option 3) from west to east was maintained through the site to provide a visual (and physical) connection across the development.

A secondary pedestrian access from north to south was also maintained from Option 2 to more closely knit the neighborhood together. A green courtyard is located off this route to further establish a sense of place for this new community.

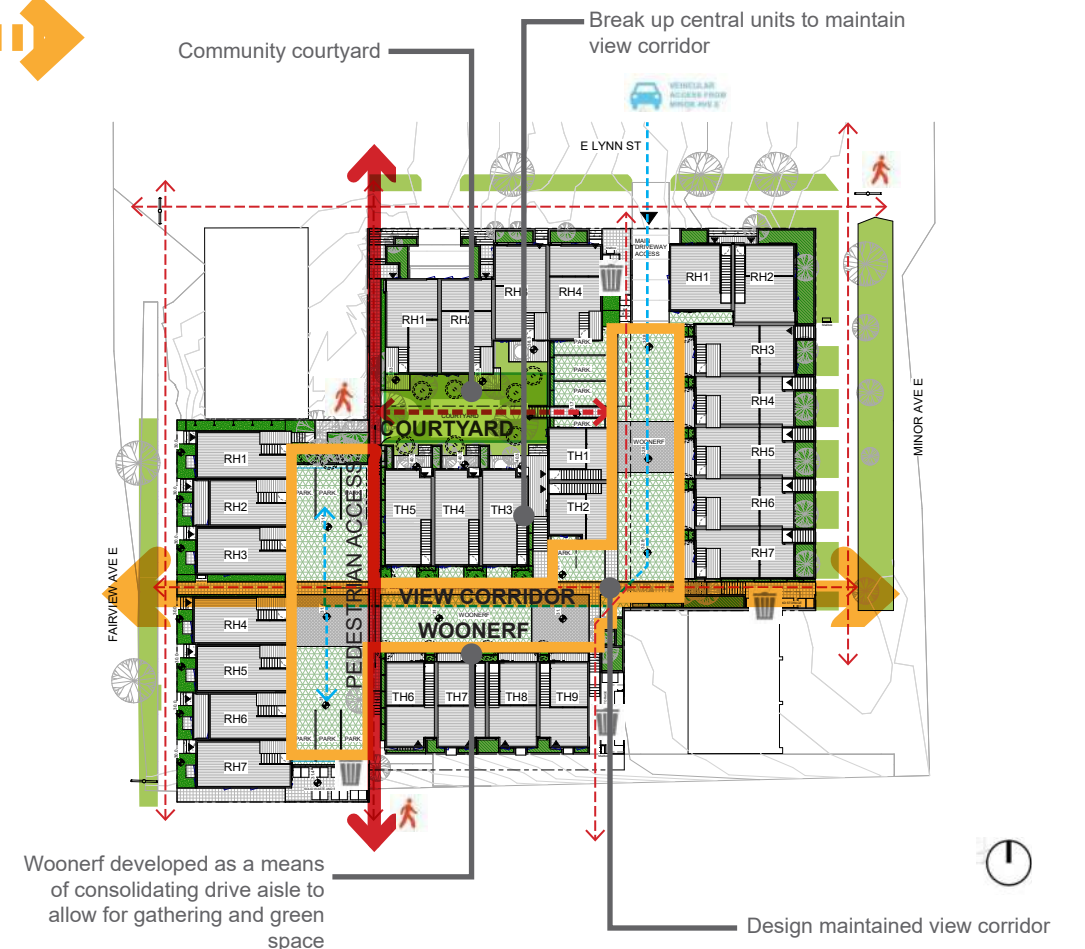
EDG Option 2 - WOONERF



EDG Option 3 - VIEW CORRIDOR



Recommendation



Woonerf



Green courtyard

High-quality, natural exterior materials

Landscape buffer / planters at each unit

Activity area at edges of woonerf

Balconies off central south units provide eyes on the woonerf and additional activity / amenity space

Large trees and landscape features to green the woonerf and open space

Large planters define parking area

Concrete pavers differentiate the main view corridor and pedestrian walkway

Grasscrete defines vehicular access / greens the ground level

Generous open spaces allow for light, air and community activity and interaction throughout the developed design.

EDG GUIDANCE:

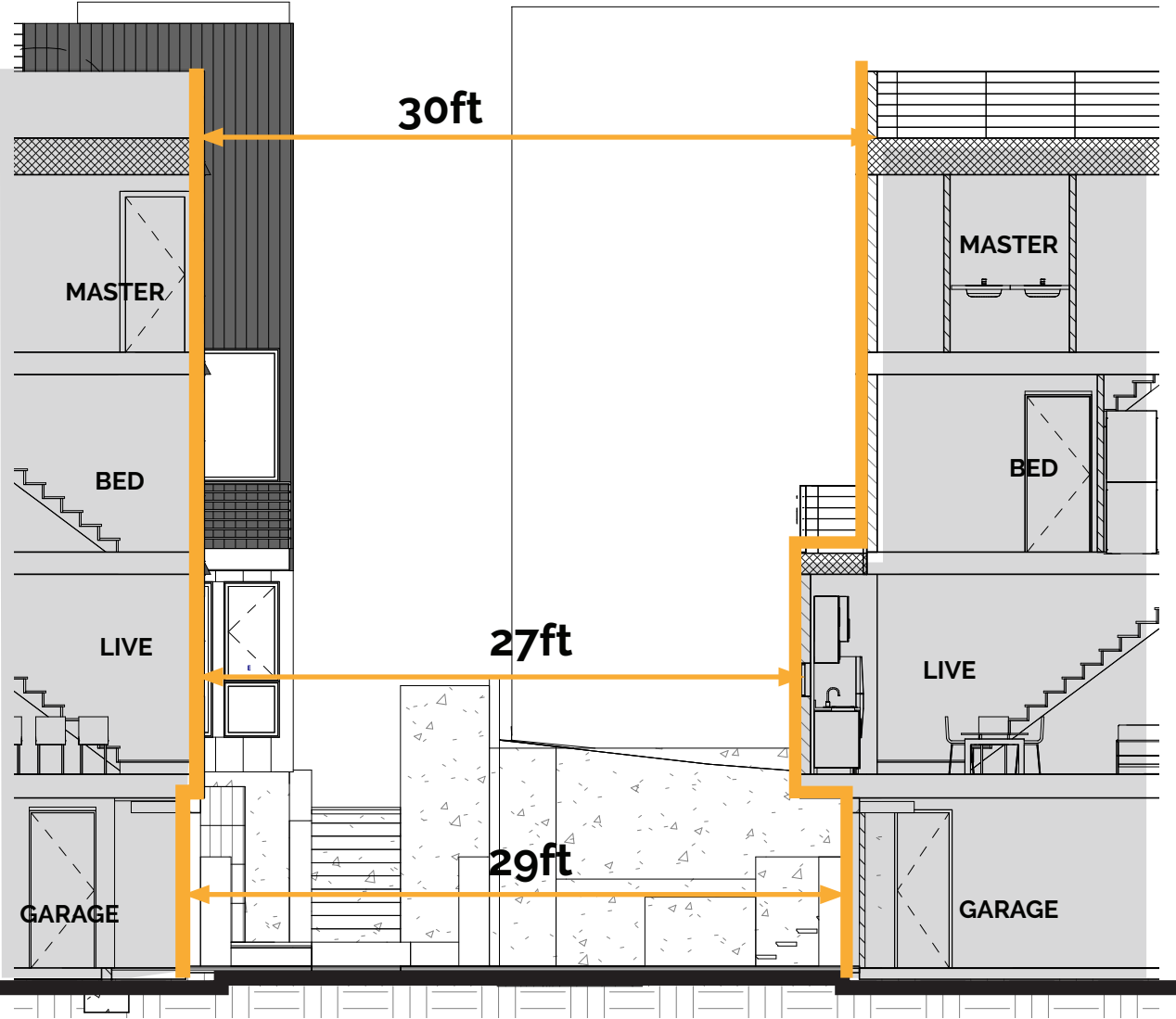
3. Open Space and Street Engagement:

c. The Board recommended pushing the units facing Fairview Avenue East further back from the street maximizing greenspace and vegetation along the designated neighborhood greenway. (PL1-A-1 Enhancing Open Space)

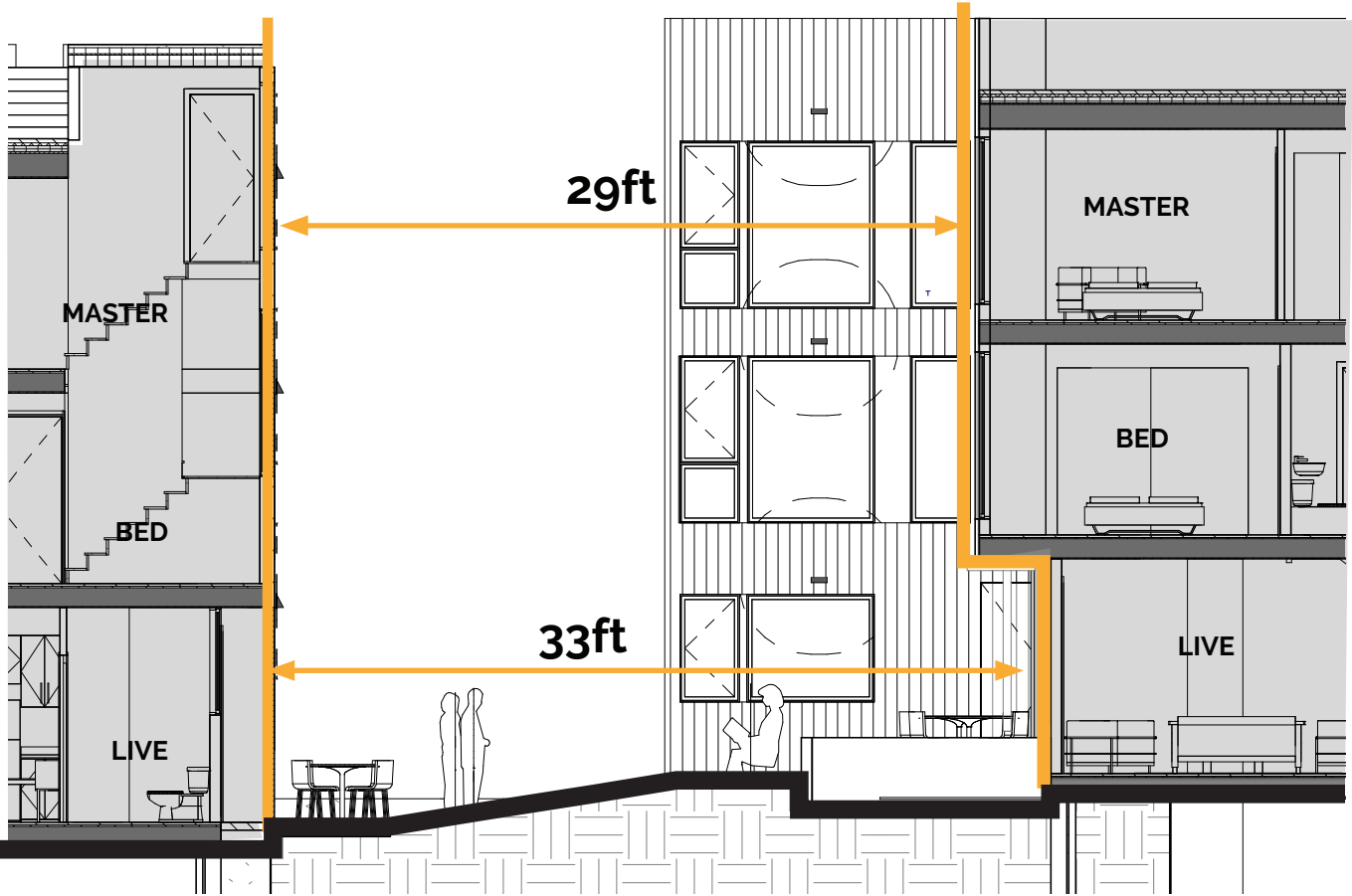


Woonerf Precedent

Woonerf



Courtyard



Courtyard



Balconies beyond - eyes on the courtyard

Seating areas are design to allow for informal community gatherings and interaction

Natural wood material softens the internal character of the building mass

Covered entry stoops into unit provide overhead weather protection

Vegetation and plantings help buffer pedestrians from residence and create texture against the building's materiality

Raised decks create variety and layering and seperate public from more private residential spaces

Planters at deck edge buffer against the more public courtyard and help define the semi-private deck space of the unit

Courtyard and Woonerf Materiality



Grass Crete



Dock leading to nearby houseboats



Vegetative Buffer



Layered Planting



Lynn St Park



Grasscrete in Woonerf



Dock floor boards



Planting with color and texture



Soft ground transitions



Community Amenities

The board supported maximizing green space and vegetation along the street edge to enhance the neighborhood greenway along Fairview Ave E.

EDG GUIDANCE:

3. Open Space and Street Engagement:

c. The Board recommended pushing the units facing Fairview Avenue East further back from the street maximizing greenspace and vegetation along the designated neighborhood greenway. (PL1-A-1 Enhancing Open Space)

RESPONSE:

c. To articulate the massing along Fairview Ave E, each unit has been pulled or pushed to define the individuality of each unit within the block. A layered landscape plan was also developed at the ground level to maximize the vegetative buffer along the sidewalk edge and further enhance the greenway along the street. Raised entry stoops welcome visitors and owners and create a sense of arrival to the units. Further recesses are carved out on the third floor to create balconies and additional eyes on the street.

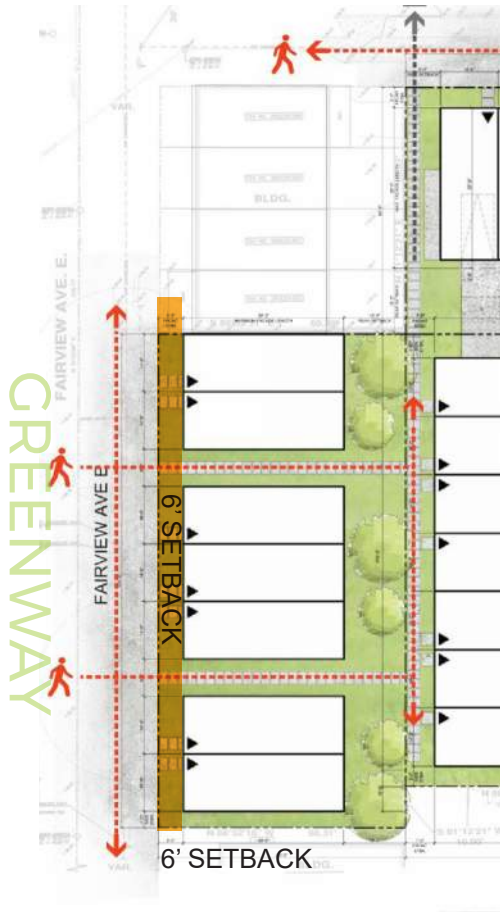


THIRD FLOOR SETBACK TO CREATE BALCONIES

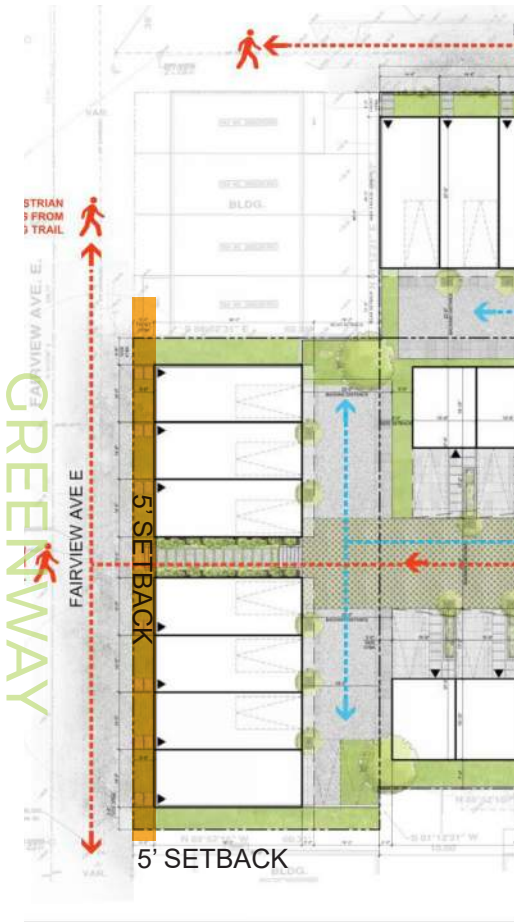
STAGGERED MASSING MODULATION

RAISED ENTRY STOOPS W/ LAYERED PLANTING BUFFER

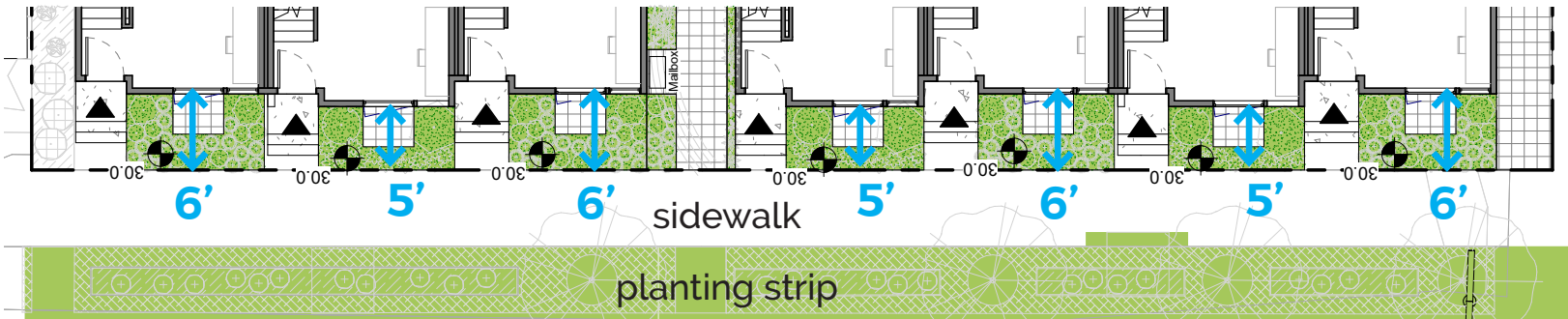
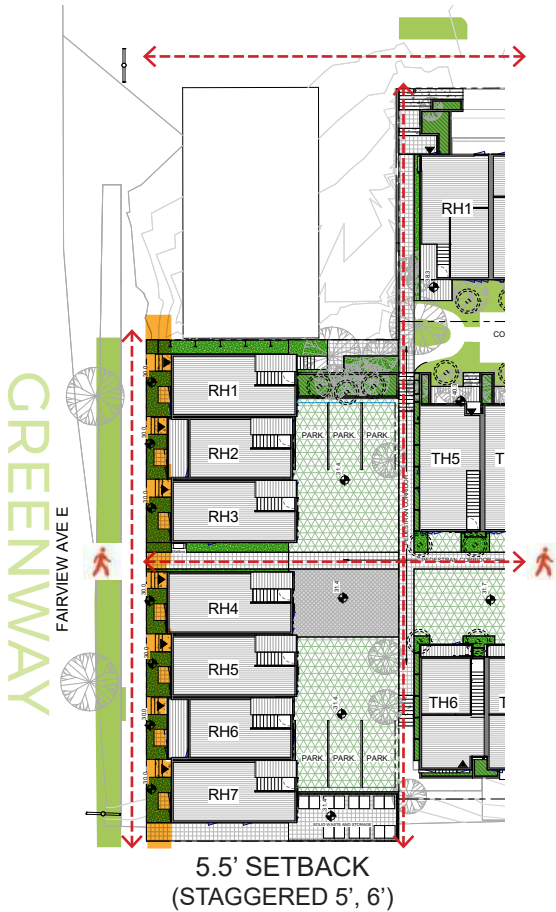
EDG Option 2



EDG Option 3



REC



FAIRVIEW AVE E

RECYCLING / WASTE ENCLOSURES ④



The board encouraged exploration of multiple trash enclosures throughout the development in lieu of a singular trash enclosure servicing all units.

EDG GUIDANCE:

4. Recycling / Waste Enclosures:

Hearing public concern, the Board encouraged exploration of multiple trash enclosure locations versus one location as presented at EDG. The Board strongly encouraged accommodated internal collection per the recommendation of SDOT. The Board also suggested the applicant investigate the impacts of having a singular trash enclosure on neighboring properties. (DC1-C-4 Service Uses)

RESPONSE:

Multiple waste enclosures have been designed throughout the development on each parcel. Enclosures will be screened through landscape and fencing and will be easily accessed by the residential units.



TRASH SCREENING PRECEDENT



1
RECYCLING /
SOLID WASTE
ENCLOSURES

PROJECT DRAWINGS

Basement - Parking Level

27 units, 27 parking spots
units 16-17.5 ft wide - Fairview & Lynn
units 16-17.5ft wide - Minor & Interior
(4) stories w/ minimized excavation
reduced paving and mix of garage &
surface parking



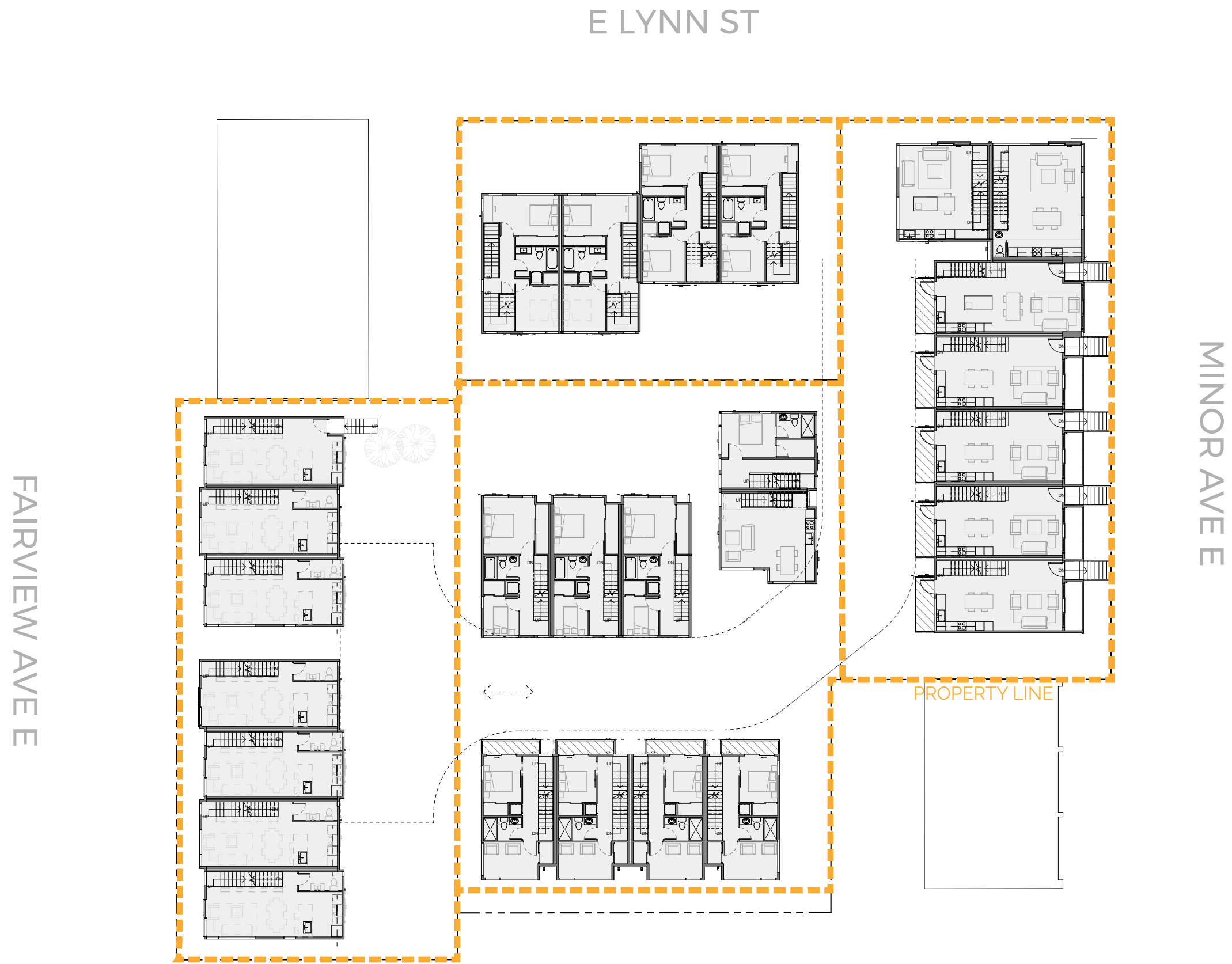
WINDOW WELL
1'-10" MIN FROM
PROPERTY LINE

Level 1 Floor Plan

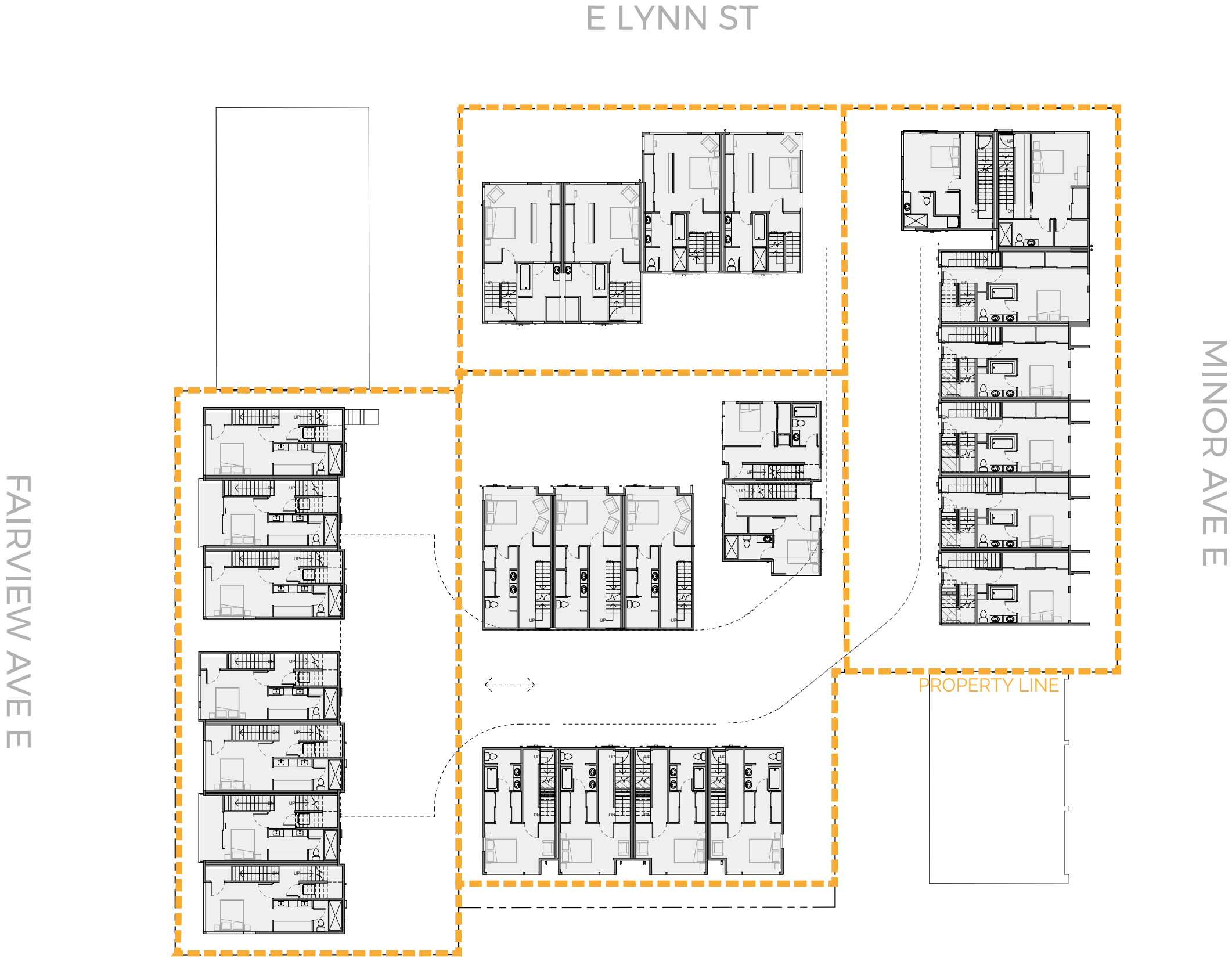


RECYCLING /
SOLID WASTE
ENCLOSURES

Level 2 Floor Plan



Level 3 Floor Plan



Amenity Spaces



COURTYARD RENDERING



WOONERF RENDERING



Landscape Plan



PLANT SCHEDULE	
TREES	BOTANICAL NAME / COMMON NAME
	Acer circinatum / Vine Maple
	Acer griseum / Paperbark Maple Street Tree
	Cornus nuttallii / Western Flowering Dogwood
	Existing Tree
	Magnolia x laeblumii Merrill / Merrill Magnolia Street Tree
	Pink Pom Poms Redbud / Cercis canadensis Pink Pom Poms Street Tree
	Pinus contorta 'Contorta' / Shore Pine
	Populus tremuloides / Quaking Aspen
	Rhamnus purshiana / Cascara
GROUND COVERS	
	Ajuga reptans / Bugleweed
	Arctostaphylos uva-ursi 'Vancouver Jade' / Kimikintick
	Cornus canadensis / Bunchberry Dogwood
	Fragaria chiloensis / Beach Strawberry
	Pachyandra terminalis / Japanese Spurge
	Rubus calycinoides Emerald Carpet / Creeping Raspberry

PLANT SCHEDULE	
SHRUBS	BOTANICAL NAME / COMMON NAME
	Acorus gramineus 'Ogon' / Golden Variegated Sweetflag
	Beesia deltophylla / Beesia
	Carex comans 'Frosty Curis' / New Zealand Hair Sedge
	Carex morrowii 'Ice Dance' / Ice Dance Japanese Sedge
	Dryopteris erythrosora / Autumn Fern
	Leucothoe fontanesiana 'Rainbow' / Rainbow Leucothoe
	Pieris japonica 'Branwen's Beauty' / Lily of the Valley Bush
	Rhododendron x 'Ramapo' / Ramapo Rhododendron
	Sarcococca nuscifolia / Fragrant Sarcococca
BIORETENTION	
	Acorus gramineus 'Ogon' / Golden Variegated Sweetflag
	Carex abrupta / Slaughter Sedge
	Cornus alba 'Saucu' / Goldenleaf Dogwood
	Cornus sericea / Red Osier Dogwood
	Juncus effusus / Soft Rush
	Sambucus nigra 'Black Lace' / Black Lace Elderberry
NATIVE SHRUBS	
	Blechnum spicant / Deer Fern
	Cornus sericea 'Kelsey' / Kelsey's Dwarf Red-Osier Dogwood
	Deschampsia cespitosa / Tufted Hair Grass
	Dicentra formosa / Pacific Bleeding-Heart
	Gaultheria shallon / Salal
	Iris tenax / Oregon Iris
	Mahonia aquifolium 'Compacta' / Compact Oregon Grape
	Mahonia nervosa / Low Oregon Grape
	Mahonia repens / Creeping Oregon Grape
	Mahonia x media 'Charity' / Mahonia
	Myrica californica / Pacific Wax Myrtle
	Polystichum munitum / Western Sword Fern
	Rosa nutkana / Nootka Rose
	Vaccinium ovatum / Evergreen Huckleberry

Rendered Lighting Plan



L1 STEP LIGHT



L2 BOLLARD LIGHT



L3 LANDSCAPE LIGHT



L5 SCONCE



Signage and Wayfinding



DIRECTIONAL SIGNAGE



● ADDRESSING SIGNAGE



Fairview Ave E Building - Exterior Elevations

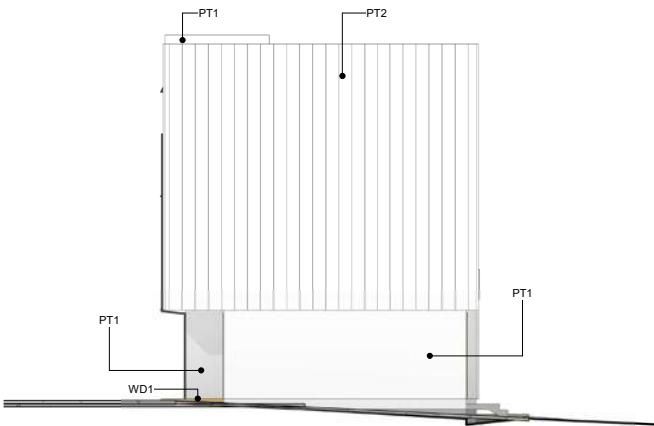
NOTE: DRAWINGS ARE NOT TO SCALE FOR THIS PRESENTATION



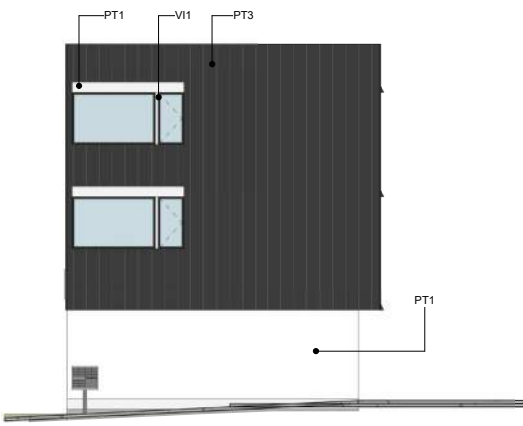
4 | FAIRVIEW WEST DR
1/8" = 1'-0"



1 | FAIRVIEW EAST DR
1/8" = 1'-0"



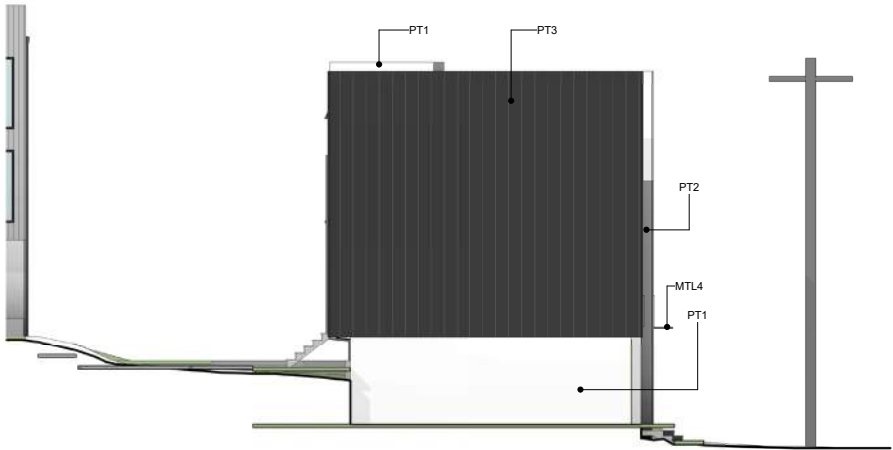
5 | FAIRVIEW CORRIDOR NORTH DR
1/8" = 1'-0"



6 | FAIRVIEW CORRIDOR SOUTH DR
1/8" = 1'-0"



3 | FAIRVIEW SOUTH DR
1/8" = 1'-0"



2 | FAIRVIEW NORTH DR
1/8" = 1'-0"

MATERIAL LEGEND

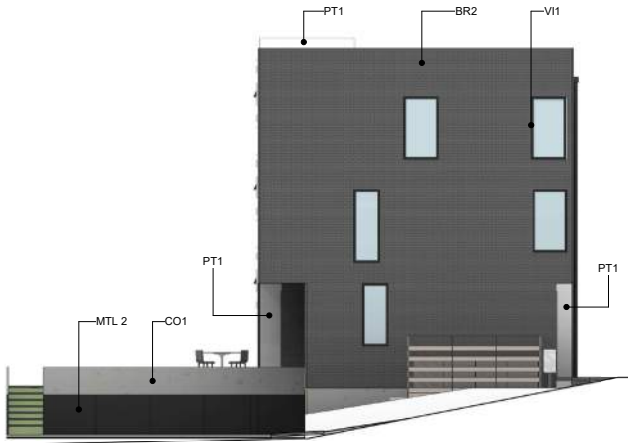
- BR #1 THIN BRICK GRAY
- BR #2 THIN BRICK DARK GRAY
- BR #3 THIN BRICK MAUNA LOA
- PT #1 HARDI PLANK SIDING - WHITE
- PT #2 VERTICAL HARDI BOARD & BATTEN - WHITE
- PT #3 VERTICAL HARDI BOARD & BATTEN - BLACK
- WD #1 VERTICAL CEDAR SIDING - NATURAL
- WD #2 VERTICAL CEDAR SIDING - GRAY
- WD #3 VERTICAL CEDAR SIDING - BLACK
- MTL #1 BOX RIBBED - WHITE
- MTL #2 METAL PANEL - DARK GRAY
- MTL #3 POWDER COATED ALUMINUM - YELLOW
- MTL #4 POWDER COATED ALUMINUM - DARK GRAY
- VI #1 VINYL MULLION - WHITE
- VI #2 VINYL MULLION - BLACK
- VI #3 VINYL MULLION - YELLOW
- CO #1 ARCH. FINISH CONCRETE

E Lynn St Building - Exterior Elevations

NOTE: DRAWINGS ARE NOT TO SCALE FOR THIS PRESENTATION



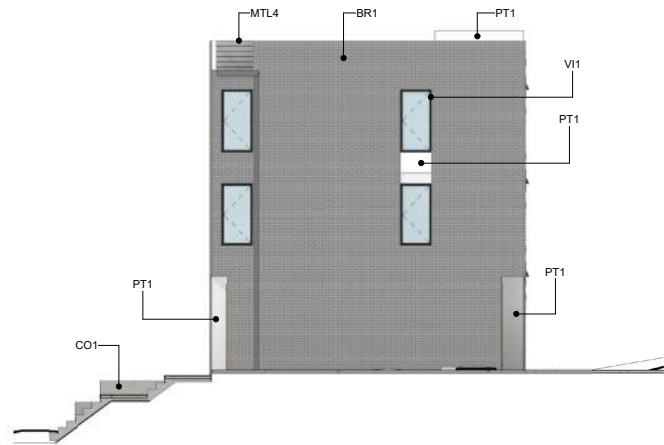
1 | North Elevation
1/8" = 1'-0"



2 | East Elevation
1/8" = 1'-0"



3 | South Elevation
1/8" = 1'-0"



4 | West Elevation
1/8" = 1'-0"

MATERIAL LEGEND

- BR #1 THIN BRICK GRAY
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- BR #3 THIN BRICK MAUNA LOA
- PT #1 HARDI PLANK SIDING - WHITE
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- MTL #2 METAL PANEL - DARK GRAY
- MTL #3 POWDER COATED ALUMINUM - YELLOW
- MTL #4 POWDER COATED ALUMINUM - DARK GRAY
- VI #1 VINYL MULLION - WHITE
- VI #2 VINYL MULLION - BLACK
- VI #3 VINYL MULLION - YELLOW
- CO #1 ARCH. FINISH CONCRETE

Minor Ave E Building - Exterior Elevations

NOTE: DRAWINGS ARE NOT TO SCALE FOR THIS PRESENTATION



1 | West Elevation
1/8" = 1'-0"



2 | South Elevation
1/8" = 1'-0"



3 | North Elevation
1/8" = 1'-0"



4 | East Elevation
1/8" = 1'-0"

MATERIAL LEGEND

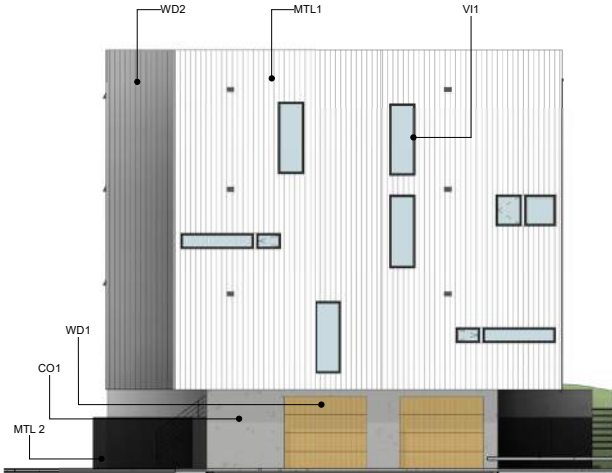
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- MTL #2 METAL PANEL - DARK GRAY
- MTL #3 POWDER COATED ALUMINUM - YELLOW
- MTL #4 POWDER COATED ALUMINUM - DARK GRAY
- VI #1 VINYL MULLION - WHITE
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- VI #3 VINYL MULLION - YELLOW
- CO #1 ARCH. FINISH CONCRETE

North Central Building - Exterior Elevations

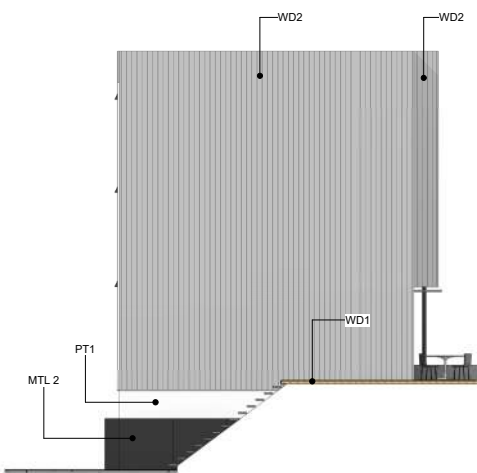
NOTE: DRAWINGS ARE NOT TO SCALE FOR THIS PRESENTATION



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1/8" = 1'-0"



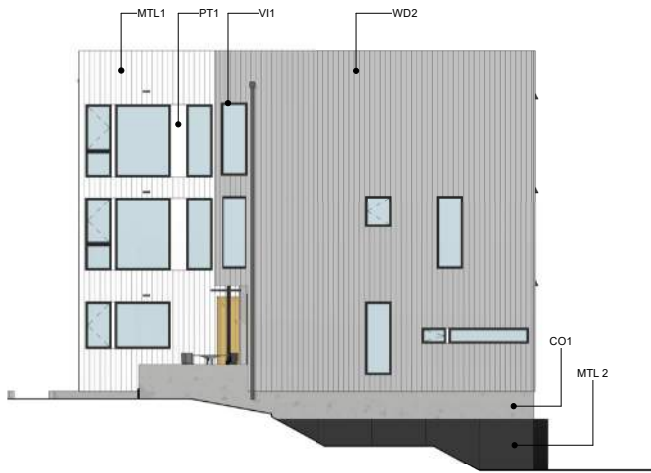
2 | Central N Bldg East Elevation DR
1/8" = 1'-0"



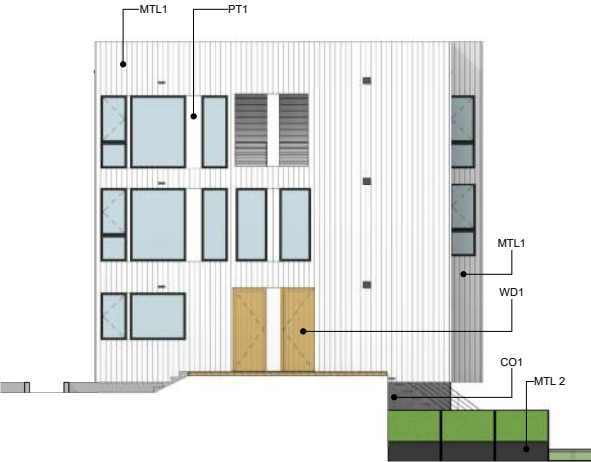
3 | Central North Building East Elevation Corridor
1/8" = 1'-0"



4 | Central N Bldg South Elevation DR
1/8" = 1'-0"



5 | Central N Bldg West Elevation DR
1/8" = 1'-0"



6 | Central North Building West Elevation Corridor
1/8" = 1'-0"

MATERIAL LEGEND

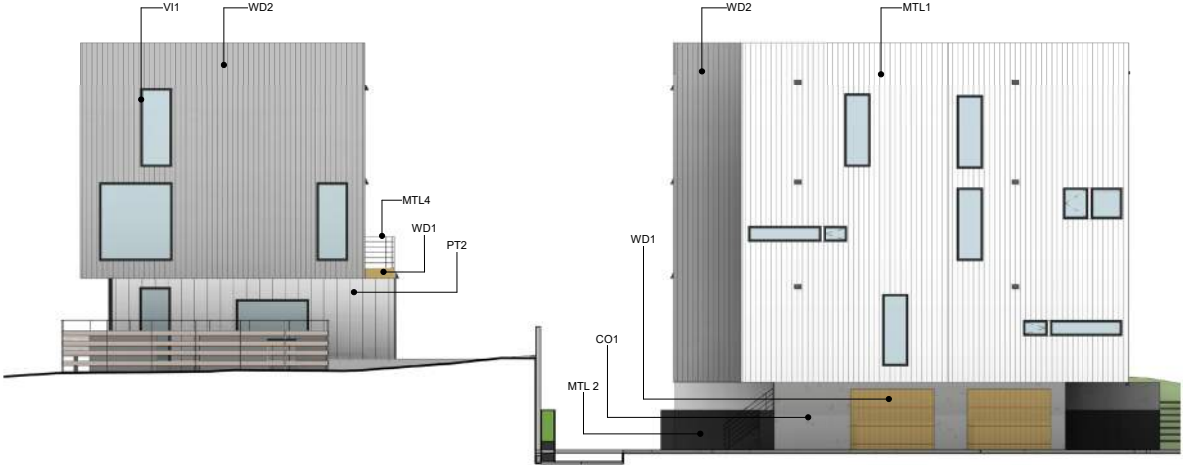
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- BR #3 THIN BRICK MAUNA LOA
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- WD #3 VERTICAL CEDAR SIDING - BLACK
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Central South Building - Exterior Elevations

NOTE: DRAWINGS ARE NOT TO SCALE FOR THIS PRESENTATION



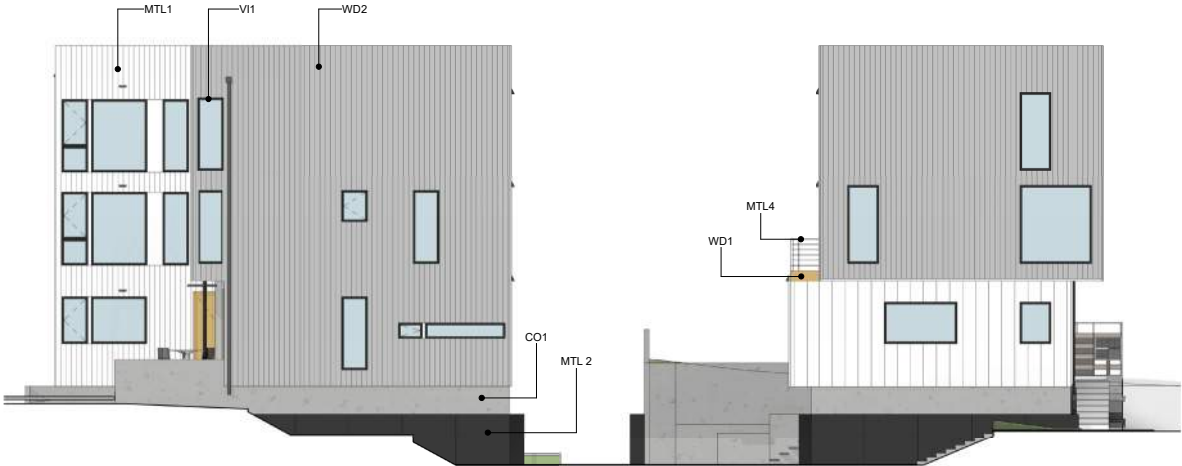
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2 | Central S Bldg East Elevation DF
1/8" = 1'-0"



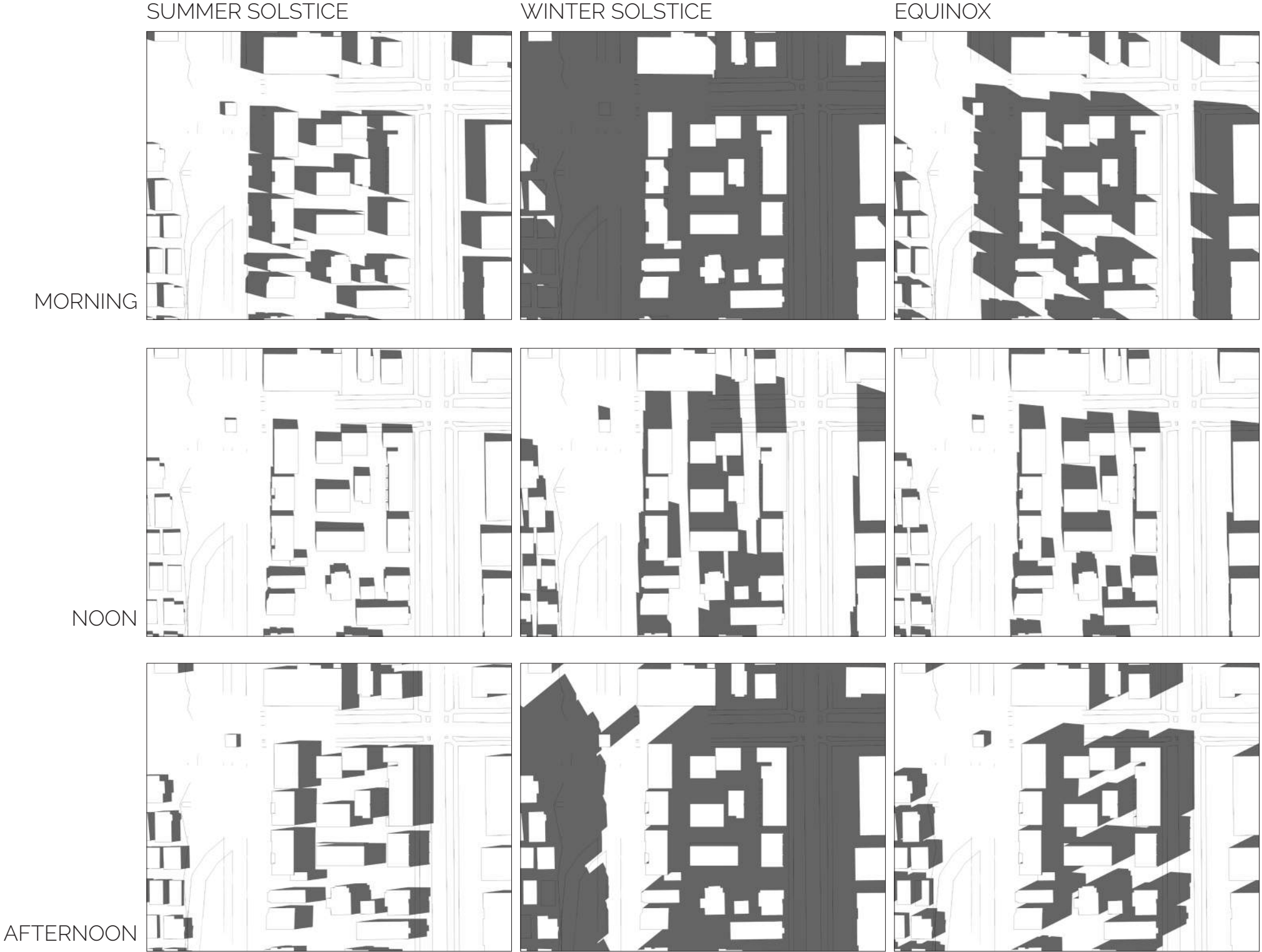
3 | Central S Bldg South Elevation DR
1/8" = 1'-0"



4 | Central S Bldg West Elevation DR
1/8" = 1'-0"

MATERIAL LEGEND

- BR #1 THIN BRICK GRAY
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- WD #3 VERTICAL CEDAR SIDING - BLACK
- MTL #1 BOX RIBBED - WHITE
- MTL #2 METAL PANEL - DARK GRAY
- MTL #3 POWDER COATED ALUMINUM - YELLOW
- MTL #4 POWDER COATED ALUMINUM - DARK GRAY
- VI #1 VINYL MULLION - WHITE
- VI #2 VINYL MULLION - BLACK
- VI #3 VINYL MULLION - YELLOW
- CO #1 ARCH. FINISH CONCRETE



DEPARTURES

DEPARTURE MATRIX

DEPARTURE	CODE REQUIRED	REQUEST	DESIGN GUIDELINES	RATIONALE
1. Amenity Space Reduction (23.45.522.A.2)	<i>(23.45.522.A.2): The Code requires a minimum of 50 percent of the required amenity area shall be provided at ground level, except that amenity area provided on the roof of a structure that meets the provisions of subsection 23.45.510.E.5 may be counted as amenity area provided at ground level. For Option 2, the applicant proposes to reduce the required the amenity space at ground level by 62% on Parcel C (from 1088 sq. ft. to 412 sq. ft.).</i>	Amenity space calculation to allow Woonerf area to be included in amenity area calculation for projects at 2247 Minor and 2236 Fairview. Per 23.45.522.D.6, a woonerf may provide maximum of 50% of amenity area if the design of the woonerf is approved through a design review process.	CS2-B3, Character of Open Space, PL1-B1, Pedestrian Infrastructure	<i>Projects at 2236 Fairview and 2247 Minor consolidates driveway access to adjacent sites and contain a required turn around (for the benefit of all four sites) that protects the neighborhood greenway by preventing a second curb cut on Fairview.</i>
2. Driveway Width (23.53.025.D)	<i>(23.53.025.D): The Code requires vehicle access easements serving ten or more residential units, easement width shall be a minimum of 32' and provide a surfaced roadway at least 24' wide. Applicant proposes the driveway be a minimum of 12'</i>	Proposed drive aisle is 20'-0" wide. Departure required for a 12'-0" curb cut at Lynn St Rowhouses and for primary drive aisle access off Lynn St.	DC1-B1, Vehicular Access and Circulation DC1-C1,2 Parking and Service Uses	<i>To minimize the visual impact of vehicular access on site and provide additional space at the pocket park, the drive aisle is proposed to have a 20'-0" paved surface. Additional setbacks beyond 20'-0" accomodate garage entrances and are used to provide planters that will make a welcoming woonerf space" connected to each neighboring street by a pedestrian connection.</i>
3. Curb Cut Reduction	<i>(23.54.030.F.2.b): The Code requires curb cut widths for two-way traffic to be a minimum of 22'-0" and a maximum of 25'-0". (DC1-B Vehicular Access and Circulation, DC1-C-2. Visual Impacts)</i>	Departure is required to reduce the two curb cuts to 12'-0" on E. Lynn Street, an Urban Village neighborhood access, non-arterial street (Per SDOT), effecting curb cuts on projects 67 E. Lynn Street and 2247 Minor Avenue E.	CS2-B3, Character of Open Space PL1-B1, Pedestrian Infrastructure DC1-B1, Vehicular Access and Circulation	<i>To minimize the visual impact of vehicular access on site and provide additional space at the courtyard, the curb cuts have been minimized to 12'-0". It is assumed that per SMC 23.54.030.D.1.f that the E. Lynn project driveway, serving fewer than five vehicles, may back out onto the street because E. Lynn Street is not an arterial street.</i> <i>The courtyard thus is free from vehicle traffic, providing open green space and community amenity area in lieu of parking.</i>

DEPARTURE REQUEST - 1

EDG departure request and preliminary Board support:

1. Amenity Space Reduction (23.45.522.A.2): The Code requires a minimum of 50 percent of the required amenity area shall be provided at ground level, except that amenity area provided on the roof of a structure that meets the provisions of subsection 23.45.510.E.5 may be counted as amenity area provided at ground level. For Option 2, the applicant proposes to reduce the required the amenity space at ground level by 62% on Parcel C (from 1088 sq. ft. to 412 sq. ft.).

The Board indicated preliminary support for the amenity space reduction on Parcel C as it allows for the larger woonerf amenity space to be located elsewhere on the development site by consolidating the pathways and circulation results in a larger and stronger design concept for the woonerf. (DC1-A-2 Gathering Places, DC3-C-2 Amenities/Features, DC1-C-3 Multiple Uses)

Recommendation departure request:

Amenity space calculation to allow Woonerf area to be included in amenity area calculation for projects at 2247 Minor and 2236 Fairview. Per 23.45.522.D.6, a woonerf may provide maximum of 50% of amenity area if the design of the woonerf is approved through a design review process.

2236 Fairview :

Total AA required = 1818 sf
Total non-woonerf AA provided = 1544 sf >50%
Woonerf area = 700 sf
Total AA provided including Woonerf = 2244 sf

2247 Minor:

Total AA required = 1801 sf
Total Non Woonerf AA provided = 3,325 sf
Total Woonerf area = 1382 sf
Total AA provided including Woonerf = 4707 sf

Rational:

Projects at 2236 Fairview and 2247 Minor consolidates driveway access to adjacent sites and contain a required turn around (for the benefit of all four sites) that protects the neighborhood greenway by preventing a second curb cut on Fairview.

Cited Design Guidelines:

CS2-B3, Character of Open Space, PL1-B1, Pedestrian Infrastructure



DEPARTURE REQUEST - 2

EDG departure request and preliminary Board support:

2. Driveway Width (23.53.025.D): The Code requires vehicle access easements serving ten or more residential units, easement width shall be a minimum of 32' and provide a surfaced roadway at least 24' wide. Applicant proposes the driveway be a minimum of 12'.

The Board indicated preliminary support for the driveway width reduction as it reduces the visual impact of the vehicular access to the development site and allows a break in the building massing of the units facing Minor Avenue East. However, the Board encouraged a pedestrian connection from each street. (DC1-B Vehicular Access and Circulation, DC1-C-2. Visual Impacts)

Recommendation departure request:

Proposed drive aisle is 20'-0" wide. Departure required for a 12'-0" curb cut at Lynn St Rowhouses and for primary drive aisle access off Lynn St.

Rational:

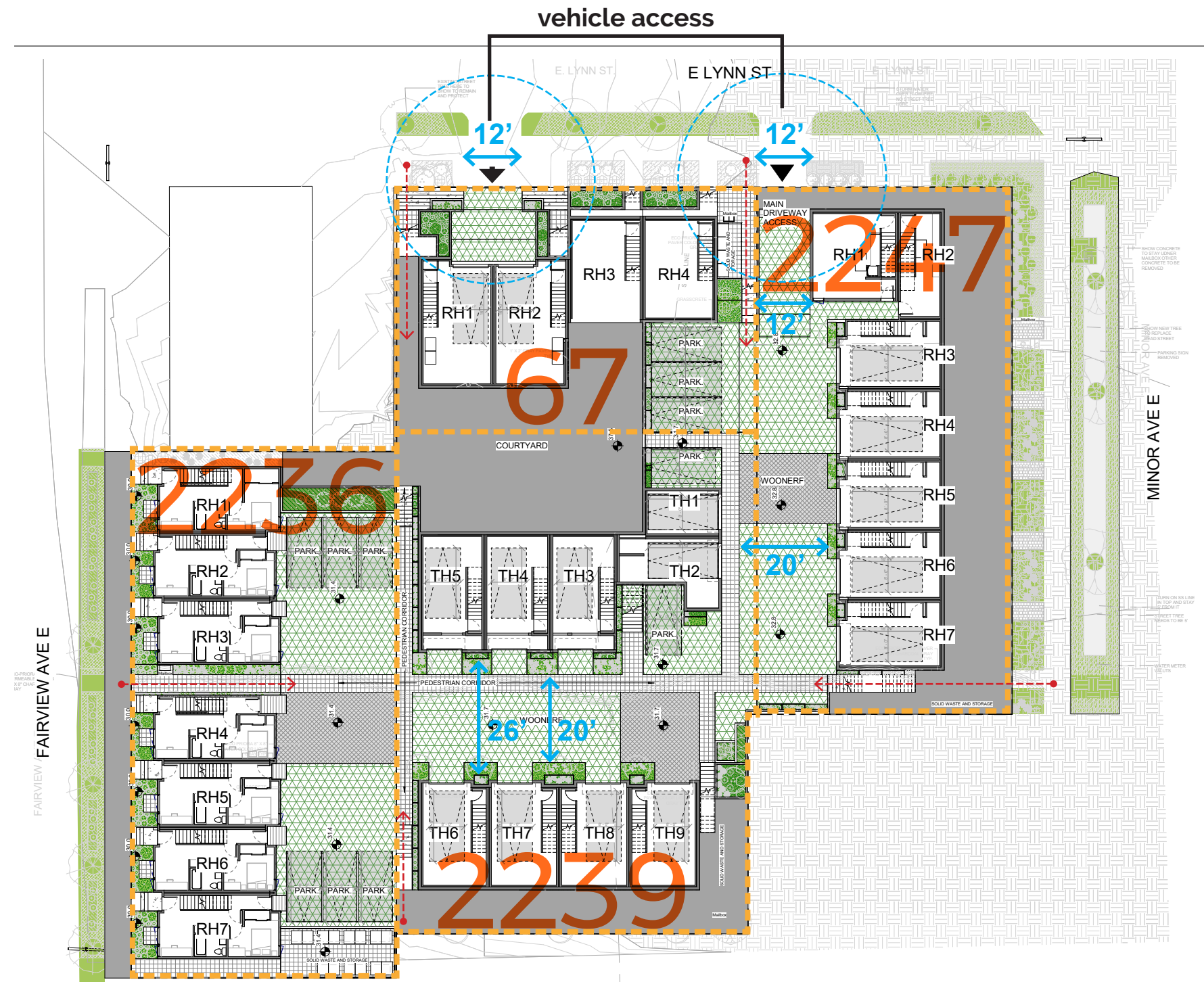
To minimize the visual impact of vehicular access on site and provide additional space at the pocket park, the drive aisle is proposed to have a 20'-0" paved surface. Additional setbacks beyond 20'-0" accommodate garage entrances and are used to provide planters that will make a welcoming "woonerf space" connected to each neighboring street by a pedestrian connection.

(Note that the drive aisle will be used by residents and will not be required for fire department access to interior units.)

Cited Design Guidelines:

DC1-B1, Vehicular Access and Circulation

DC1-C1,2 Parking and Service Uses



Note: Per SDOT street type standards, E Lynn St is classified as an urban village neighborhood access, non-arterial street (2.10)

DEPARTURE REQUEST - 3

Departure Request

3. **Curb Cut Reduction (23.54.030.F.2.b):** The Code requires curb cut widths for two-way traffic to be a minimum of 22'-0" and a maximum of 25'-0". (DC1-B Vehicular Access and Circulation, DC1-C-2. Visual Impacts)

Recommendation departure request:

Departure is required to reduce the two curb cuts to 12'-0" on E. Lynn Street, an Urban Village neighborhood access, non-arterial street (Per SDOT), effecting curb cuts on projects 67 E. Lynn Street and 2247 Minor Avenue E.

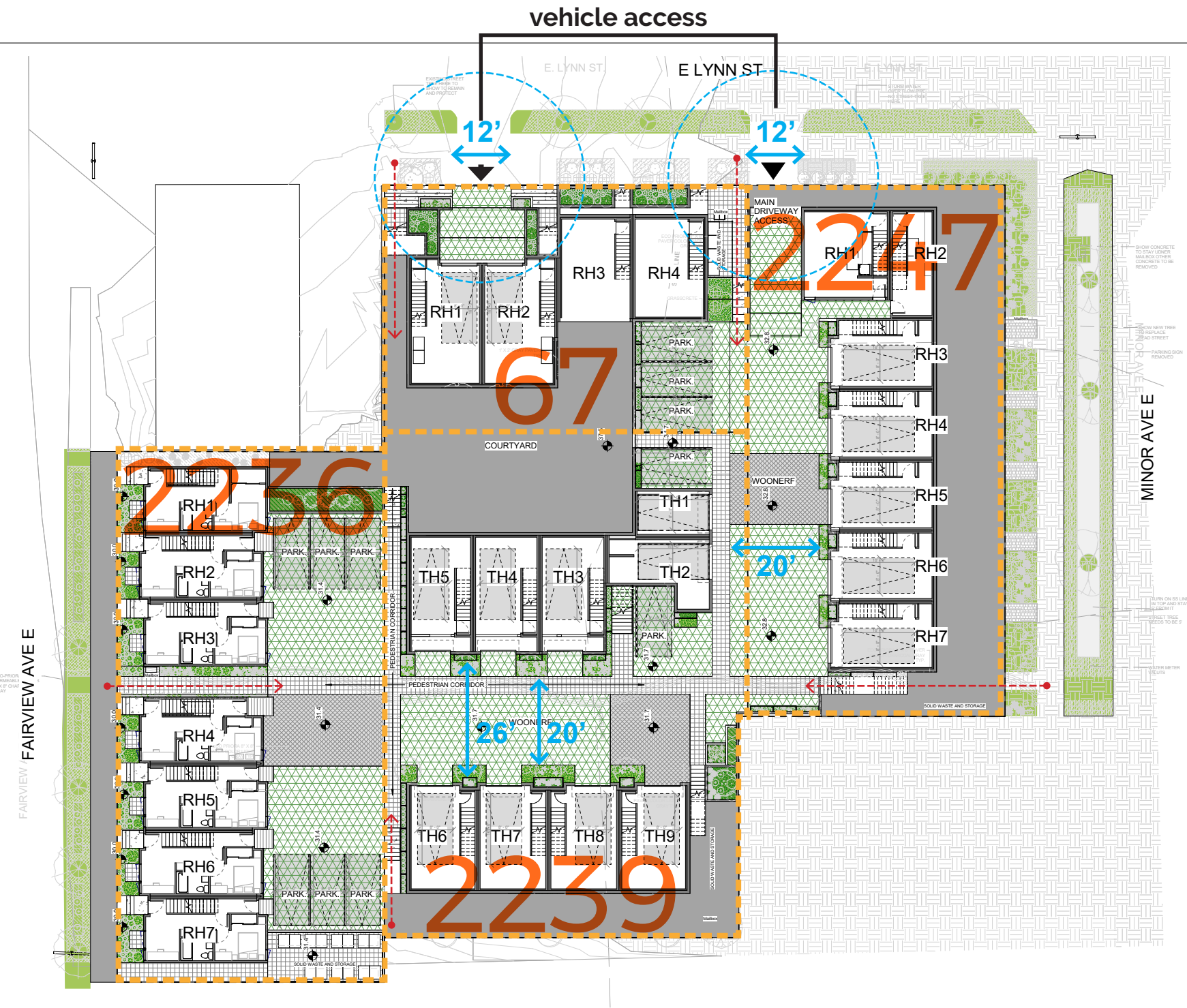
Rational:

To minimize the visual impact of vehicular access on site and provide additional space at the courtyard, the curb cuts have been minimized to 12'-0". It is assumed that per SMC 23.54.030.D.1.f that the E. Lynn project driveway, serving fewer than five vehicles, may back out onto the street because E. Lynn Street is not an arterial street.

The courtyard thus is free from vehicle traffic, providing open green space and community amenity area in lieu of parking.

Cited Design Guidelines:

CS2-B3, Character of Open Space
PL1-B1, Pedestrian Infrastructure
DC1-B1, Vehicular Access and Circulation

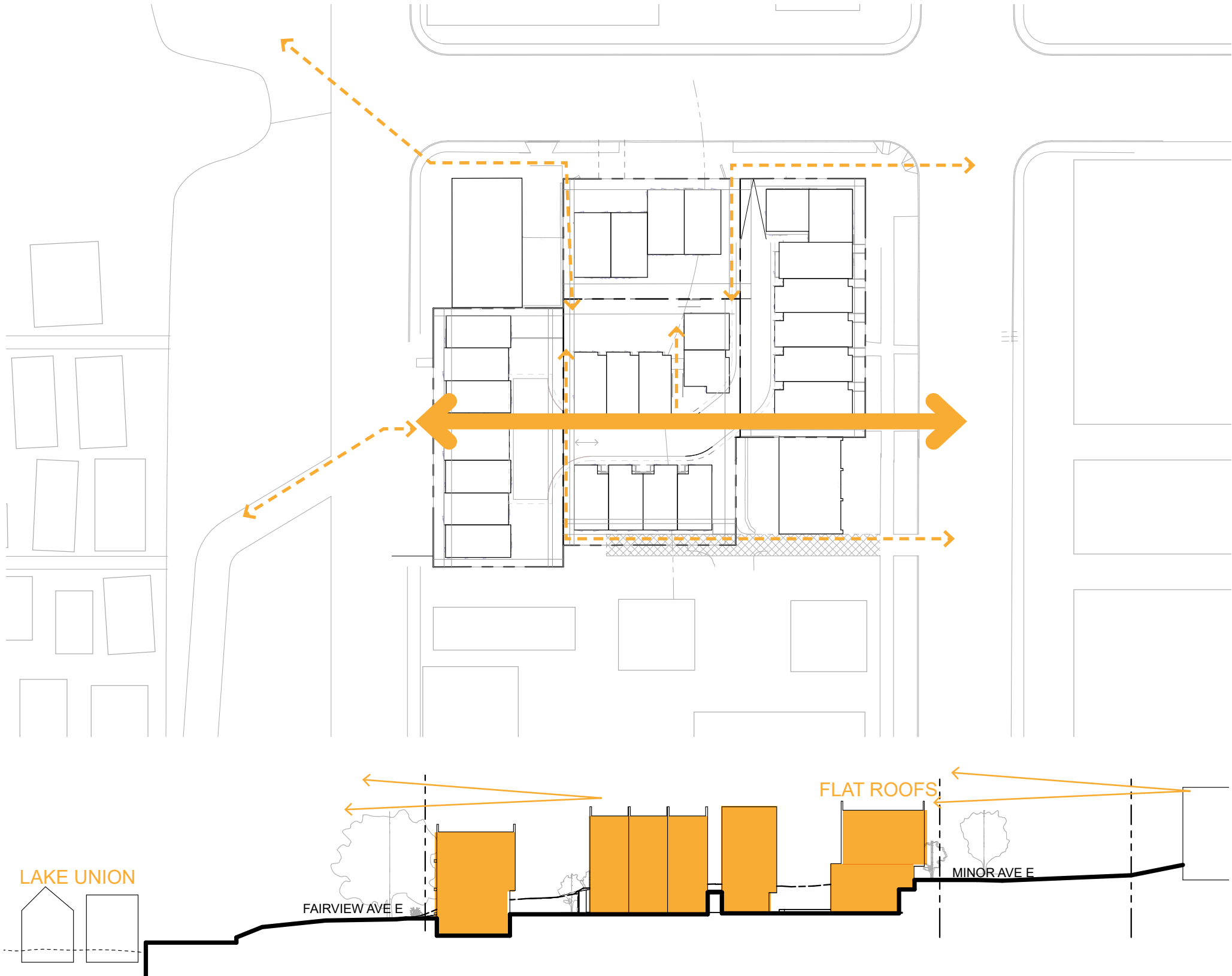


Note: Per SDOT street type standards, E Lynn St is classified as an urban village neighborhood access, non-arterial street (2.10)

Public comments supported flat roofs, articulation to break up street frontages and design features to enhance the sidewalk.

PUBLIC COMMENT RECEIVED AT EDG:

- Supported creating a view corridor through the site by breaking up the massing of the units along Minor Avenue East. - **View corridor maintained**
- Supported the woonerf concept and the idea of adding some color to the project. - **Woonerf concept maintained**
- Concerned with the affordability of the units.
- Supported the use of articulation to break up the façade along all street frontages. - **Each street facade provides unique articulation opportunities.**
- Concerned with the contrast of the proposed architectural style with that of the existing neighborhood, the lack of street engagement throughout the project, and the uninviting entries to the units. - **Entries include stoops and planters to engage with the sidewalks.**
- Concerned with the monolithic nature of the units. Would support the introduction of some variety in the units and façades. - **Each street facade broken into minimum of 2 different masses with articulation within each mass.**
- Supported incorporating units with flat roofs, open staircases, and open railing along Minor Avenue East. - **Flat roofs and open railings are provided along Minor Ave E**
- Concerned with the centralizing of the garbage. - **Garbage is decentralized. Each site is provided a separate waste collection location.**
- Concerned with the lack of design features on the units facing the street.- **Details, such as stoops, balconies, planters are added to each entry.**
- Supported enhancing the sidewalk.
- Neighbor whom shares driveway easement requested to be made a party of record.
- Concerned with the impact of views to Lake Union. - **To respect views of Lake Union, units are provided with flat roofs and open railings facing Minor.**
- Opposed the use of gable roofs for the townhouses on site. - **Gable roofs are eliminated.**



PUBLIC COMMENT

The following public comments were offered at this meeting:

• Supported creating a view corridor through the site by breaking up the massing of the units along Minor Avenue East. *Response: view corridor maintained between Minor Ave E and Fairview.*

• Supported the woonerf concept and the idea of adding some color to the project. *Response: Woonerf concept maintained. Color is added through planting and architectural details - like overhead canopies and doors.*

• Concerned with the affordability of the units.

• Supported the use of articulation to break up the façade along all street frontages. *Response: Plane and color changes break up mass of building along all street frontages.*

• Concerned with the contrast of the proposed architectural style with that of the existing neighborhood, the lack of street engagement throughout the project, and the uninviting entries to the units. *Response: Proposed architectural style is modern, however the materiality is meant to connect with the existing neighborhood through the use of brick, wood and lap siding.*

• Concerned with the monolithic nature of the units. Would support the introduction of some variety in the units and façades. *Response: Each street facing façade is addressed individually, to add variety to the facades and units. Massing breaks and material changes break up longer street facing facades at Fairview and Minor.*

• Supported incorporating units with flat roofs, open staircases, and open railing along Minor Avenue East. *Response: Street facing row houses along Minor have flat roofs and open staircases to reduce the impact of project on neighbors access to views.*

• Concerned with the centralizing of the garbage. *Response: Garage has been decentralized. Each site has a collection site.*

• Concerned with the lack of design features on the units facing the street. *Response: Street facing façades planters and stoops are oriented to engage the sidewalk. Facades will be articulated with railings, operable windows and material changes.*

• Supported enhancing the sidewalk.

SDCI staff also summarized design related comments received in writing prior to the meeting:

• Neighbor whom shares driveway easement requested to be made a party of record.

• Concerned with the impact of views to Lake Union. *Response: To respect views to Lake Union, higher penthouses have been removed. Flat roofs with open stairs are proposed.*

• Opposed the use of gable roofs for the townhouses on site. *Response: Flat roofs are proposed.*

The following comments from the Seattle Department of Transportation were submitted to SDCI in writing prior to the meeting:

• Seattle Municipal Code (SMC) requires street trees along the three public right-of-way frontages of the project site. SDOT prefers all three frontages to equal 11.5 feet, with a six-inch curb, five-foot planting strip with street trees, and a six-foot concrete sidewalk. Larger dimensions beyond the standard are also desirable, though not the curb width. SDOT would like to better understand the dimensions proposed for all frontages.

Response: Proposed street dimensions Lynn Street is bordered by 5ft planting strip with street trees, and 6ft concrete sidewalk. Minor Ave E maintains existing 11ft planting strip with 6ft sidewalk. Fairview Ave E is upgraded to include a 6ft sidewalk and 5-6ft planting strip per SIP approval.

• SDOT agrees with vehicular access off Minor Avenue East or E Lynn Street as Fairview Avenue East, as stated in the packet, has been considered as a neighborhood greenway with expectations for as low as possible number of vehicle trips. SDOT also prefers trash/recycling/compost collection to occur on private property, rather than container staging in the rights-of-way, when feasible.

Response: Trash collection is decentralized. Lynn and Fairview will be serviced by individual caddies and Minor and interior units will require combined waste, staged in the R.O.W.

PRIORITIES & BOARD RECOMMENDATIONS

After visiting the site, considering the analysis of the site and context provided by the proponents, and hearing public comment, the Design Review Board members provided the following siting and design guidance.

1. MASSING & DESIGN CONCEPT

The Board discussed the merits of massing Options Two and Three, analyzing the open space on site and how the massing typology of the residential units fit into the surrounding neighborhood character. The Board believed the layout configuration and inclusion of the woonerf in Option Two provided the best opportunity to create a successful open space on site, as well as, help to establish a sense of community for the project. Also discussed in detail was the view corridor found in Option Three and how it integrated with the neighborhood and pedestrians traveling along Fairview Avenue East. After a thoughtful discussion, the Board supported moving forward with development of Option Two, with adherence to the following guidance:

Response: The proposed site plan incorporates the board supported woonerf from Option 2 and the view corridor from Option 3. Additional open space for communal use added Lynn St row houses is completely separated from cars. In response to public comment about the lack of parking spaces, 6 additional parking places were also added, providing parking

to units along Fairview.

a. Acknowledging public comment, the Board discussed the use of the gable roof form as a mechanism for reducing the perceived massing of the structures and as a method for establishing character for the individual units throughout the site. The Board recommended the applicant explore areas where the gable roof can be most successful and areas where the flat roof form is more appropriate in terms of providing views to Lake Union and adding some variety to the proposal. (CS2-D Height, Bulk, and Scale)

Response: To preserve Lake views for these townhouses and their uphill neighbors, flat roofs are proposed. Public comment also discouraged the use of gables and encouraged flat roofs. Open stairs are used in place of stair penthouses to reduce height. To add and establish character of the individual units, each street front is given a different character through form and materiality. The height of the street facing facade varies. The form of Fairview most closely resembles a repeating, classic townhouse form with lap siding. Minor Ave Rowhouses are a modern form with recessed entry doors and lofted spaces. Units along Lynn and wrapping the corner of Lynn and Minor Ave are more massive buildings, resembling the apartments around, with brick siding.

b. E Lynn St. The Board echoed public comment and strongly recommended incorporating more variation (changing rooflines, façade modulation, etc.) in the townhouses located along E Lynn Street to break up the monotony of the massing. The Board acknowledged the topography created some variety due to the slope, however, commented that greater variety was needed. The Board suggested exploring the following with the goal of creating a sense of individuality, identity, and character (CS1-C Topography, CS2-B Adjacent Sites, Streets, and Open Spaces, CS2- C-3 Full Block Sites, DC1-A-2 Gathering Places):

i. The Board recommended locating some, or all, of the roof decks for the townhouses along E Lynn Street and Minor Avenue East toward the street to create variety in massing modulation. (CS2-B-2 Connection to the Street, PL2-B Safety and Security)

Response: Roofdecks for units along Lynn alternate between the front half and back half of the roof to give variety along the street and to provide more privacy between neighbors.

ii. The Board strongly recommended incorporating the use of color in the material palette to help establish a sense of character for the proposal. (CS3-A Emphasizing Positive Neighborhood Attributes)

Response: Color is incorporated in the detail elements of the project such as handrails, door overhangs and doors.

iii. The Board suggested the applicant explore areas where secondary architectural features can be utilized to break up the massing and provide some detail to the townhouses. (DC2-A-2 Reducing Perceived Mass , DC2-C Secondary Architectural Features)

Response: To reduce townhouse masses, project uses projected masses, recessed entry stairs, carved upper stories

for balconies, gaps in parapets for railings.

iv. The Board referenced the precedent image shown on page 39 of the EDG packet which used subtle variety in the composition of windows and modulation along with color to create variation and identity for each unit. (DC2-B-1. Façade Composition)

Response: Units include subtle differences in overhead canopy, color, numbers, and modulation that add to the variety of each townhouse façade and distinguish the units.

c. Fairview Ave. The Board was supportive of the 2-3-2 building grouping as this created a successful residential scale and rhythm. (CS3-A-4 Evolving Neighborhoods)

Response: a 3-4 grouping along Fairview was preferable to provide wider units, a more generous opening between townhouses that incorporates a view corridor from Minor Ave E and allow for space on either side of the project for structure and trash access. Modulation of the facades along Fairview further break down the 3-4 grouping.

2. RESIDENTIAL ENTRIES:

a. The Board expressed their concerns with the parking/entry sequence for the interior units of the proposal. At the next meeting, the Board would like clarification of how the entries for the interior units will be treated. (PL3-A Entries, PL3-B Residential Edges)

Response: Townhouses in the center of the site have been provided both a front and back door. The ‘front door’ is next to the garage entries and may be used by the residents. The “back door” is located along a more green space, and might be the entrance used by a guest or when a resident returns home by foot.

b. The concept of the sunken patios adjacent to entries along the street frontages intrigued the Board, however, the Board expressed concern for the fully sunken condition and was more comfortable with the partially sunken patio condition. At Recommendation, the Board would like to see more details related to this condition. (PL3-A Entries, PL3-B Residential Edges)

Response: The proposal has eliminated sunken patios along street frontages in exchange for raised stoops.

c. The Board agreed that the use of fencing around the project should not be used as a method for providing security to the units. The Board recommended exploring the use of transitional spaces to help further distinguish the entry areas and to provide a buffer from public to private, while staying engaged with the public realm. (PL3-B-1 Security and Privacy)

Response: Fences are used in select locations to provide screening for waste collection. Otherwise, planting and raised stoops distinguish entrances and provide separation from the street.

d. The Board acknowledged public concern about interacting with the street and sidewalk and strongly supported the use of stoops along the street frontage. This is especially important for the units that are easily accessible from the

sidewalk. (PL3-A Entries, PL3-B Residential Edges, DC2-D-1 Human Scale)

Response: Stoops are used at all street frontages to distinguish the residential entry from the sidewalk.

3. OPEN SPACE AND STREET ENGAGEMENT:

a. The Board agreed with public comment and expressed strong support for the woonerf concept, highlighting its potential to create a successful communal space that would be utilized by the residents. (PL1-A Network of Open Spaces, DC1-A-2 Gathering Places , DC1-C-3 Multiple Uses, DC4-D Trees, Landscape, and Hardscape Materials)

Response: A woonerf located between Lynn Rowhouses and the central townhouses is expanded to include small pocket green spaces to encourage use as a gathering space. The pocket park serves as both an extended back yard to units fronting the park and includes designated public spaces with hardscaping, benches, and bbqs.

b. The Board agreed with public comment and encouraged the applicant to explore incorporating the view corridor present in Option Three into the design of Option Two and possibly connecting it to the proposed woonerf. This would involve providing a break in the layout of the central units. The Board expressed they would be open to a departure on Parcel C if this would assist in accommodating the view corridor and result in a stronger tie to the neighborhood. (CS2-B Adjacent Sites, Streets, and Open Spaces, PL1-B Walkways and Connections, PL1-C-1 Selecting Activity Areas)

Response: The view corridor present in Option 3 is incorporated into the proposed plan. Parking access along the view corridor is widened to make space for additional planters and entry stoops that add to softness to the corridor. Grass crete is used in large swaths to reduce the impact of large paved areas.

c. The Board recommended pushing the units facing Fairview Avenue East further back from the street maximizing greenspace and vegetation along the designated neighborhood greenway. (PL1-A-1 Enhancing Open Space)

Response: Fairview units stagger in distance from the sidewalk to create variety of massing and interest at the sidewalk.

4. RECYCLING AND WASTE ENCLOSURES:

Hearing public concern, the Board encouraged exploration of multiple trash enclosure locations versus one location as presented at EDG. The Board strongly encouraged accommodated internal collection per the recommendation of SDOT. The Board also suggested the applicant investigate the impacts of having a singular trash enclosure on neighboring properties. (DC1-C-4 Service Uses)

Response: The project has incorporated on trash collection location per site. Fairview and Lynn rownhouses will have individual trash collection, per unit, while the two Minor sites will have combined trash. This reduces the impact of trash on any one location on site, makes trash more accessible (and therefore more likely to be utilized).

HYBRID Previous Project Experience



Madison Park Condominiums



Bellevue Ave Midrise Apartments



w Court Townhouses



Stevens Residences



Mullet House

